

City of Alexandria, Virginia

MEMORANDUM

DATE: SEPTEMBER 3, 2025

TO: CHAIR AND MEMBERS OF THE
BOARD OF ARCHITECTURAL REVIEW

FROM: HISTORIC PRESERVATION STAFF

SUBJECT: CONCEPT REVIEW OF 330 NORTH FAIRFAX STREET
BAR CASE # 2025-00305

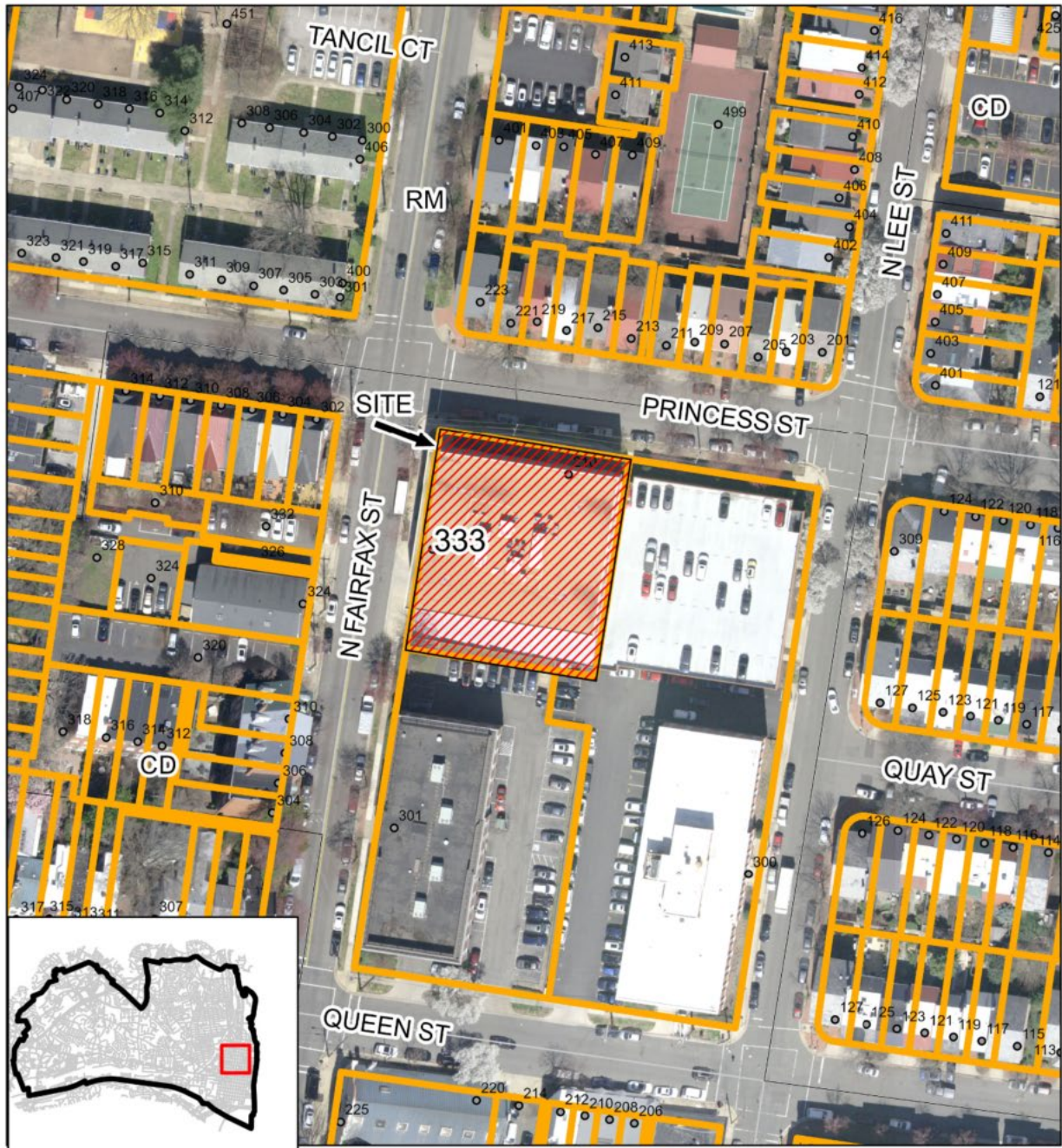
I. SUMMARY

The applicant is requesting BAR Concept Review of the redevelopment of the property at 330 North Fairfax Street, to include the construction of a new multi-unit townhouse development in place of the existing office buildings. The proposed development will encompass the majority of the block, with the exception of the site of the previously approved multi-unit residential building at 301 North Fairfax Street.

The Concept Review Policy was adopted in May 2001 and amended and restated in 2016 (attached). Concept Review is an optional, informal process at the beginning of a Development Special Use Permit (DSUP) application whereby the BAR provides the applicant, staff, Planning Commission, and City Council with comments relating to the overall appropriateness of a project's height, scale, mass, and general architectural character. These comments are not binding on the BAR or the applicant. The Board takes no formal action at the Concept Review stage but will provide comments and may endorse the direction of a project's design by a straw vote. If the Board believes that a building height or mass, or area proposed for construction is not appropriate and would not be supported in the future, the applicant and staff should be advised as soon as possible. This early step in the development review process is intended to minimize future architectural design conflicts between what is shown to the community and City Council during the DSUP approval and what the Board later finds architecturally appropriate under the criteria in Chapter 10 of the Zoning Ordinance and the BAR's adopted *Design Guidelines*.

The Development Special Use Permit (CDSUP#2025-00004) associated with this project will be heard by Planning Commission and City Council at a time to be determined. The following approvals will be requested through this process:

1. A DSUP for 32 cluster townhomes in the CD Zone
2. An SUP for a cluster development
3. An SUP for bonus density for the provision of affordable housing
4. An SUP for lots without public street frontage
5. Yard, lot area, and lot width modifications from the CD zone standards per the cluster SUP.
6. A proposed subdivision



BAR2025-00305
333 N Fairfax Street



0 40 80 160 Feet

II. SITE CONTEXT AND HISTORY

Site Context

The project site includes the majority of the block bounded by North Fairfax Street, Princess Street, North Lee Street, and Queen Street, with the exception of the multi-unit residential building previously approved for the southwest corner of the block. The proposed configuration of the townhouse blocks will create interior alleys and pedestrian pathways, the result of which is that all parts of the proposed townhouses will be visible from a public right of way.

History

This block has included a variety of uses over the years, including industrial, residential, and entertainment. The southwest corner of the block has been dominated by industrial use, with foundry and wood cutting buildings shown in this area on the 1885 Sanborn Insurance Map. A variety of different industrial uses existed in this area until the construction of an office building in 1977.

The southeast corner and eastern edge of the block included single unit residential, boarding houses, and small retail uses as early as 1885. As shown on the 1941 and 1959 Sanborn Insurance Maps, these small scale residential style structures were replaced with industrial uses. The office building currently located at 300 North Lee Street was approved by the BAR at the August 12, 1964 hearing. At that time, the building was described as a “warehouse-office building on the west side of Lee Street between Princess and Queen Streets.”

According to the July 1996 edition of the Alexandria Archaeology *Volunteer News*, Irish immigrant Dan Henry constructed a saloon at the southeast corner of Princess and North Fairfax Street shortly after the Civil War. In 1893, the building was expanded with the construction of a large addition to being used as a theater and concert hall. The structure housed numerous musical and theatrical events but became known for hosting boxing matches and other sporting events. The saloon and theater were closed prior to 1941 when the Sanborn Insurance Map shows the site having been incorporated into the industrial use to the south of the site. The “Essex Building” commercial office building currently occupying the site was constructed in **1979**.

III. PROPOSED DEVELOPMENT

The applicant proposes to demolish the existing office buildings at 300 North Lee Street, reviewed under a separate BAR number, and 333 North Fairfax Street to construct a townhouse development in their place. The development will include 32 three story townhouses, mid block pedestrian pathways, and private drives. Each townhouse will be three stories with a fourth floor setback from the street facing façade, creating a rooftop terrace for each unit. Each unit will feature a garage accessed by a private drive. The townhouse development is divided into four different architectural types, based upon the existing adjacent architecture. The Type 1 townhouses will face Queen Street and will be red brick with projecting bays and punched windows. The Type 2 townhouses will face N Fairfax Street and will be grey brick with ganged windows. Type 3 townhouses will face into the interior courtyards and will be a combination of stone and brick with metal railings at the second floor. The Type 4 townhouses will face N Lee Street and are the most contemporary in design.

IV. STAFF ANALYSIS

As a reminder, the BAR's purview in this concept review work session is limited to endorsing the project and providing feedback on its height, scale, mass, and general architectural character. The applicant will ultimately return to the Board for approval of a Certificate of Appropriateness for architectural details, finishes, and colors after City Council approval of the DSUP.

Within the historic districts, the Board utilizes the *Design Guidelines* to determine if a potential new building or additions would be compatible with nearby buildings of historic merit. This is an unusual project in that the proposal is for a grouping of 32 individual townhouses that includes a variety of site conditions and nearby architectural context. When reviewing this project, the Board should consider the proposal at various scales. At the largest scale, the townhouses will be divided into 8 blocks arranged around the site (Figure 1). The Board should consider the proposed arrangement that has been designed to prioritize the creation of a cohesive street wall on N Fairfax Street and Queen Street. The adjacent context in these areas is more historic than the other streets, consisting mostly of single family residential buildings aligned at the edge of the sidewalk with minimal curb cuts. The properties on the opposite side of Princess Street include twentieth century townhouses, each of which has a curb cut and garage facing the street. In this area the proposed development includes end units and entries to private drives. The east side of N Lee Street consists of end units of the twentieth century townhouse development leading to Union Street. In reaction to this context, the proposal includes a single curb cut with end units making up the southern half of the block and a grouping of five townhouses facing N Lee Street at the northern end of the block. Staff finds the proposed site organization to be an appropriate response to the nearby context.

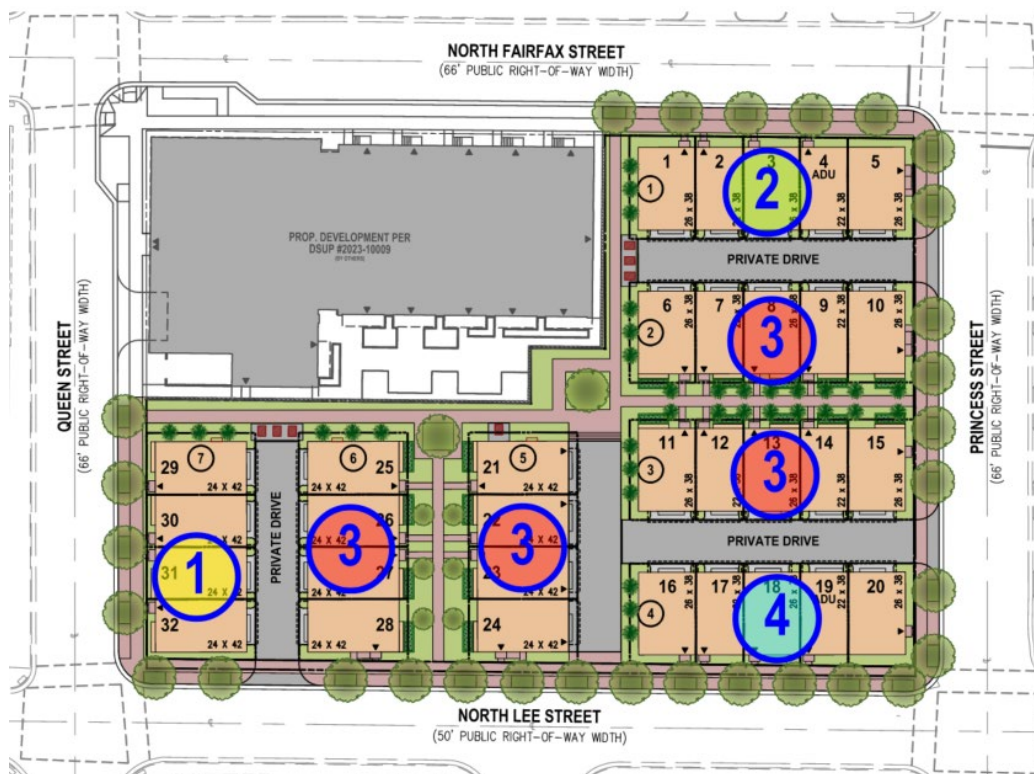


Figure 1: Proposed organization of townhouses

Type 1 Townhouses: Facing Queen Street

As noted above, the proposal prioritizes the design for the townhouses facing Queen Street for a continuous street wall and acknowledges this as one of the two adjacent streets with intact significant historic fabric. This block of four townhouses will also be directly adjacent to the previously approved multi-unit building at 301 N Fairfax Street. This portion of the site includes a significant grade change with grade going down as it moves from west to east. In response to this grade change, the townhouses step down to follow the grade with each one including an entry door adjacent to the sidewalk.

In response to the nearby historic fabric, this block of townhouses is more traditional in design than others in the development. The overall composition is two townhouse units mirrored to create a configuration that places projecting bays at the east and west ends of the block (Figure 2). The two center units have a solid brick parapet at the top of the third floor while the two end units have an open rail in this location. The effect is that the middle units appear to be taller than the end units.

Each townhouse is rendered in red brick with a projecting bay clad in grey panels. The bay begins at the second floor and includes three adjoining windows at the second and third floors. A recessed building entry door is located at the edge of the unit with a stack of single windows above. The fourth floor is recessed, creating a roof deck for each unit. The fourth floor is clad in a grey panel similar to the cladding on the projecting bays. These panels extend to the ground on the east and west ends of the townhouse block, centered on the elevation. The rear of this block of townhouses is similarly rendered in a red brick for the first three floors with a grey paneled fourth floor. The ground floor at the rear of each townhouse is dominated by a garage entry door and a second floor projecting balcony with a metal guard rail.



Figure 2: Rendering of the Type 1 townhouses facing Queen Street

Type 2 Townhouses: Facing North Fairfax Street

Along with the townhouses facing Queen Street, the block of townhouses facing N Fairfax Street have been acknowledged as being closest to historic fabric. It should be noted that the buildings directly across N Fairfax Street from these townhouses are not historic in nature. These buildings include a commercial building and modern townhouses that face Princess Street. However, the buildings at the south end of the west side of the block are historic two story townhouses. This block of buildings will also be located adjacent to the previously approved multi-unit building at 301 N Fairfax Street (Figure 3).



Figure 3: North Fairfax Street elevation of 301 North Fairfax Street

Given that the buildings directly across the street from this block of townhouses are modern and the design for the building at 301 N Fairfax Street is also, the applicant is proposing a design that includes aspects of traditional architecture and a more contemporary design (Figure 4). The buildings will be clad in a grey brick for the first three floors and the setback upper level will be clad in a similarly colored panel. Each townhouse includes stacked three part windows at the second and third floor above a three part ground floor window. A single width stacked window will be aligned with the recessed entry door and canopy. The rear of this townhouse block will include ground floor garages and a second floor balcony. Two part stacked windows will be at the second and third floors centered on a square two part window at the fourth floor.



Figure 4: Rendering of the Type 2 townhouses facing North Fairfax Street

Type 3 Townhouses: Facing Interior Courtyard

The Type 3 townhouses will be located in two locations within the middle of the block and will face one another across new interior courtyards. While these blocks will be organized with their long ends facing one another, each block will feature an end unit with a main entrance on the short side of the block facing the sidewalk. This will orient the unit towards the street, improving the pedestrian experience along the streetscape. When considering the compatibility of the proposed design for these townhouse blocks, the Board should consider the relationship of the elevation on the short side of the block to the existing building across the street. Since these blocks occur mid-block, the relevant buildings are the front of the modern townhouses on the north side of Princess Street and the side elevations of the modern townhouses on the east side of South Lee Street. The townhouses on the north side of Princess Street are brick, 3 and 4 bay buildings with side gable roofs and a garage door at the ground floor. The garage doors and related curb cuts dominate the streetscape in this block. The east side of North Lee Street in this area is bisected by Quay Street that aligns approximately at the third point of the subject block. The townhouses opposite the proposed Type 3 townhouse blocks in this area face onto Queen and Quay Streets, leaving the secondary side elevations as the dominant context for the proposed townhouses.

The design for the Type 3 townhouses is similar to the design for the Type 1 block with some minor modifications (Figure 5). In these buildings, the proposed bay extends to the ground where the stone base extends around the first floor. Three bay stacked windows occupy the second and third floors with single punched windows aligned above the recessed building entry door. Metal railings are located at the bottom of the stacked windows on the second floor of each bay. The recessed fourth floor and middle panel at the end of each townhouse block will be clad in dark panels. As with the other townhouse blocks, the rear of the buildings will include a garage entry door at the ground floor and stacked windows above.



Figure 5: Rendering of Type 3 townhouses facing the interior courtyards

Type 4 Townhouses: Facing North Lee Street

The Type 4 townhouses will be located at the northeast corner of the block, facing N Lee Street. The intersection of N Lee Street and Princess Street is dominated by twentieth century townhouses extending to the north, east, and west of the project site. As noted above, these townhouses feature ground floor parking garages and curb cuts. The typical design for these townhouses is two and three stories with punched or bay windows and varying colors of brick. Many of these townhouses have been renovated in recent years, some with a distinctly contemporary architectural language. There is little historic fabric in this area of the historic district, the boundary for which is one block north of the project site.

Given the modern nature of the surrounding buildings, the design for the Type 4 townhouses is the most contemporary style of the different types (Figure 6). The block of five townhouse units is clad in a tan brick with projecting bays that include large windows with dark colored panels at the corner. The recessed fourth floor is clad in a dark panel that extends down the center of the short side of the block. At the corner of N Lee Street and Princess Street, the projecting bay turns the corner with a single window on the north elevation. As with the Type 3 townhouses to the west of this block, the building entry for the northern townhouse is located on the short side of the block. The rear of this block will include a ground floor garage door and stacked punched windows separated by dark panels similar to those used in the window bays.



Figure 6: Rendering of Type 4 townhouses facing N Lee Street

Staff finds that the configuration of the buildings on the site is responsive to the neighboring context, providing a streetscape that is similar to that which is found in the historic district. The scale and size of the townhouse blocks are also consistent with the nearby context. The three story with setback fourth floor is similar to other modern townhouse developments that have been built within the historic districts and does not overwhelm the nearby two, three, and four story buildings.

V. STAFF RECOMMENDATION

Staff recommends that the BAR request the applicant return for a second Concept Review after addressing feedback from the Board and staff. Staff finds that the proposed design for the project is appropriate for this unique location and that with some additional study including areas discussed below, the project will be a successful re-imagining of this site.

A common element in all of the townhouse types is a recessed fourth floor that creates a roof terrace at the front portion of the building (Figure 7). This overhang includes a decorative cornice at the perimeter. In all of the townhouse types, the upper level is clad in a dark colored panel that extends down the center of the short side of the building.

Staff is concerned that the recessed fourth floors give a heavy feeling to the overall building composition. There is some variation in the extent to which the overhang projects from the adjacent wall but in all cases the depth of the overhanging roof overpowers the massing, giving it a heavy appearance that creates a horizontal proportion that is in contrast to the vertical proportions of the townhouses. With the exception of the Type 2 townhouses, the upper level cladding is a dark color which further contributes to the heavy feeling of this element and the continuity of color belies the variation in the design of the different townhouse types.

Staff recommends that the applicant explore ways in which the recessed upper levels can have a lighter visual appearance. This can include revising the structure for the overhangs to make them more delicate instead of continuing the line of the cornice from the body of the building. The applicant should also consider different color schemes for the upper levels to provide variation within the overall development and give a lighter appearance to this portion of the building.



Figure 7: Designs for recessed upper level

All of the proposed townhouses share a similar form, three stories with a continuous recessed upper level set back from the front of the building. The result of this form is that with the exception of Type 1, all of the building blocks have an unbroken cornice that extends across the length of each block (Figure 8). At the Type 1 townhouse block, the end units include a railing that turns the corner in lieu of the solid parapet on the other two units.

Staff finds that the continuous cornice line found on most of the blocks is inconsistent with the vertical variation found in rows of attached townhouses in the historic districts. It is not uncommon for townhouses to have historically been built in repeating sections but when these are more than three or so sections, there is some variation in the roof line. This gives the overall composition some visual interest and contributes to a more enlivened streetscape.

Staff recommends that the applicant explore ways in which the perceived roof line can include greater variation. The inclusion of railings in lieu of a solid parapet at the Type 1 townhouses helps to break down the massing. Staff encourages the applicant to include elements such as this in other areas of the project. This can include the use of railings in pairs or singles or some other pattern that works with the composition of each block. Another option would be to include a roof trellis that extends from the recessed section to the building parapet in some units. This would give the impression of some units being four floors and some three floors without the overwhelming massing of a true four story unit.



Figure 8: Comparison of continuous cornice and Type 1 with railings

As noted above in the discussion regarding the configuration of the townhouse blocks on the site, in a number of locations where the short end of the townhouse block faces the adjacent street the townhouse entrance has been located on the short end of the block. The purpose of this configuration is to improve the streetscape by ensuring that at no point is there a blockface without unit entries. In each of these locations, the building entrance is centered on the ground floor within the panel clad, vertical slot formed between the two brick sections. As shown in the proposed elevations, these end elevations are asymmetrical with the brick portions at different heights and the recessed fourth floor above the lower brick section (Figure 9).

Staff is concerned that these end of block elevations with entry doors are the typical end of block elevation with an entry door inserted into the vertical slot and are not composed primary elevations. Typically within the historic district, the location of the townhouse entry denotes the primary elevation with secondary elevations consisting chiefly of windows. In the proposed design, the primary elevation still appears to be on the long side of the block but with the building entrance located on the secondary elevation.

Staff recommends that the applicant revise these townhouse units such that the primary elevation is clearly facing the adjacent street with a differing secondary elevation on each of the two long sides. This could be accomplished by revising the brick detailing in these locations to create a more formal composition and by simplifying the secondary elevations. In addition to improving the streetscape, this will add variety to the long sides of the townhouse block giving them additional visual interest.



Figure 9: Examples of end elevations with townhouse entries

The block of townhouses at the northeast corner of the site faces twentieth century townhouses on the opposite side of North Lee Street and Princess Street. In response to this configuration, the applicant is proposing a style for the townhouses that is more contemporary in nature than elsewhere on the site (Figure 10). These townhouses include large horizontal windows with sections of cladding in projecting bays. The projecting bays are brick to match the adjacent walls.

Staff supports the use of a contemporary architectural language in this context, noting the absence of adjacent historical context. The use of large windows and panels along with a tan brick all further the contemporary nature of the design for these townhouses. Staff finds that the composition as submitted is successful in terms of its proportions but lacks the same level of visual interest found on other townhouse types.

Staff recommends that the applicant explore ways in which an additional level of visual interest could be attained within this design. Some potential options would be to differentiate the projecting bays through the use of an alternate brick color or the inclusion of contemporary brick detailing. Each of these options will add a level of visual layering to the composition while remaining distinctly more contemporary than other townhouse types.



Figure 10: Type 4 townhouses at the north east corner of the block

In a number of locations on each of the townhouse types, smooth panels are located in contrast to the more dominant brick portions. In some cases this material is used on a projecting bay, it is also used at all of the recessed fourth floors and the vertical slot at the short end of each townhouse block. As noted on the submitted elevations, these panels will be made from Nichiha cementitious siding. This is a type of cementitious siding that is similar to the more commonly used James Hardie siding but is available with concealed fasteners and a more consistent texture.

While the proposed Nichiha panel system is a high quality fiber cement product, staff recommends that the applicant consider the use of alternate materials in these locations. Specifically at primary or street facing elevations, the Board has routinely requested materials such as metal panels, terra cotta, stone, or other similar materials. In these locations, the panels will be directly adjacent to the public right of way and subject to direct view. These alternate materials also have an inherently greater level of visual interest due to their modular size and in some cases natural variations.

Staff recommends that the BAR requests the applicant return for a second Concept Review after addressing feedback from the Board and staff. Staff finds that the proposed design for the project is appropriate for this unique location and that with some additional study including areas discussed above, the project will be a successful re-imagining of this site.

STAFF

William Conkey, AIA, Historic Preservation Architect

Tony LaColla, AICP, Land Use Services Division Chief, Planning & Zoning

VI. CITY DEPARTMENT COMMENTS

Legend: C- code requirement R- recommendation S- suggestion F- finding

Zoning

C-1 Proposed concept review for new townhouses and supporting infrastructure will comply with zoning.

C-2 Plans and design to follow what was approved under CDS2025-00004.

Code Administration

Building permit is required

Transportation and Environmental Services

1. Comply with all requirements of CDSP2025-00004. (T&ES)
2. The Final Site Plan must be approved and released and a copy of that plan must be attached to the demolition permit application. No demolition permit will be issued in advance of the building permit unless the Final Site Plan includes a demolition plan which clearly represents the demolished condition. (T&ES)

Archaeology

F-1 Records indicate that the block began to be developed in the latter eighteenth century. By the 1790s Henry McCue had built a complex of buildings along Princess Street, between N. Fairfax St. and N. Lee St. (at the time known as N. Water St.). The McCue family operated a large tavern with support buildings. In addition to the McCue complex, by 1810 the character of the neighborhood was in place, a few large dwellings interspersed with townhouses, open lots, stables, and commercial industries. During the Civil War the Union Army occupied the frontage on Princess St. with an office and large shed on the northwest corner of the block, and a steam fire engine house on the northeast

corner of the block, which included a “sink” (i.e. privy or outhouse) to the south of the engine house.

In the post-Civil War years, a row of six townhouses fronted on Queen St. The street addresses for these dwellings spanned from 201 to 211 Queen St. Several larger dwellings and lots lined along Water St. (306, 310, 314, 316, 322, 324 Water St.), and a small complex of buildings stood on the northwest corner of the block. In the 1920s the Alexandria Fertilizer and Chemical Company acquired the northeast quadrant of the block and built a facility there. By 1940 the company had expanded its ownership of the entire half of the block (what is now 300 N. Lee St.) and proceeded to tear down all the small neighborhood of buildings there. This area was then used as a work and storage yard until the late 1960s.

- F-2. If this project is a federal undertaking or involves the use of any federal funding, the applicant shall comply with federal preservation laws, in particular Section 106 of the National Historic Preservation Act of 1966. The applicant will coordinate with the Virginia Department of Historic Resources and the federal agency involved in the project, as well as with Alexandria Archaeology
- R-1 Incorporate and interpret elements of the site history and archaeological findings into the design of the public realm with a professional archaeological consultant or qualified historian, in consultation with Staff. The site plan shall indicate themes and locations of interpretive elements such as interpretive signs, markers, specialty paving, historic features, and the like.
- a. Interpretive Signage
A professional archaeological consultant or qualified historian, in consultation with Staff, shall write text and graphically design interpretive signage using a template provided by the Office of Historic Alexandria. Once approved by the Office of Historic Alexandria/Alexandria Archaeology, install the interpretative signage prior to issuance of the Certificate of Occupancy. Alternatively, the applicant may opt to have Alexandria Archaeology write, design, fabricate, and install the interpretive signage for a one-time fee.
 - b. Interpretive Elements
A professional landscape architect or designer, in consultation with Staff, shall provide graphics, and descriptions for interpretive elements such as markers, plaques, monuments, inscriptions, specialty paving, specialty railings, historic features, and the like, prior to Final Site Plan release subject to approval by the Office of Historic Alexandria/Alexandria Archaeology and the Director of P&Z. Install the interpretative elements prior to issuance of the Certificate of Occupancy. (P&Z) (Arch)
- R-2 Hire an archaeological consultant to complete a Documentary Study and an Archaeological Evaluation. If the study identifies potentially significant resources associated with the property, the consultant shall complete a Resource Management Plan, as outlined in the City of Alexandria Archaeological Standards. Preservation measures presented in the Resource Management Plan, as approved by the City Archaeologist, will

be implemented. The Archaeological Evaluation and implementation of the Resource Management Plan shall be completed prior to submission of the Final Site Plan unless archaeological work is required in concert with demolition and construction activities, which must be demonstrated to the satisfaction of the City Archaeologist. (Archaeology)

- R-3 The Final Site Plan, Grading Plan, or any other permits involving ground disturbing activities (such as coring, grading, filling, vegetation removal, undergrounding utilities, pile driving, landscaping and other excavations as defined in Section 2-151 of the Zoning Ordinance) shall not be released until the City archaeologist confirms that all archaeological field work has been completed or that an approved Archaeological Evaluation plan and any required Resource Management Plans will be implemented to recover significant resources before or in concert with construction activities. * (Archaeology)
- R-4 Call Alexandria Archaeology (703/746-4399) two (2) weeks before the starting date of any ground disturbance so that an inspection or monitoring schedule for City archaeologists can be arranged. The language noted above shall be included on all Final Site Plan sheets involving any ground disturbing activities. (Archaeology)
- R-5 Call Alexandria Archaeology immediately (703-746-4399) if any buried structural remains (wall foundations, wells, privies, cisterns, etc.) or concentrations of artifacts are discovered during development. Work must cease in the area of the discovery until a City archaeologist comes to the site and records the finds. The language noted above shall be included on all Final Site Plan sheets involving any ground disturbing activities. (Archaeology)
- R-6 The applicant/developer shall not allow any metal detection to be conducted on the property, or allow independent parties to collect or excavate artifacts, unless authorized by Alexandria Archaeology. Failure to comply shall result in project delays. The language noted above shall be included on all Final Site Plan sheets involving any ground disturbing activities. (Archaeology)
- R-7 The final Certificate of Occupancy shall not be issued for this property until the final archaeological report has been received and approved by the City Archaeologist.*** (Archaeology)
- C - 1 All required archaeological preservation measures shall be completed in compliance with Section 11-411 of the Zoning Ordinance.

VII. ATTACHMENTS

- Application Materials



INDEX OF DRAWINGS

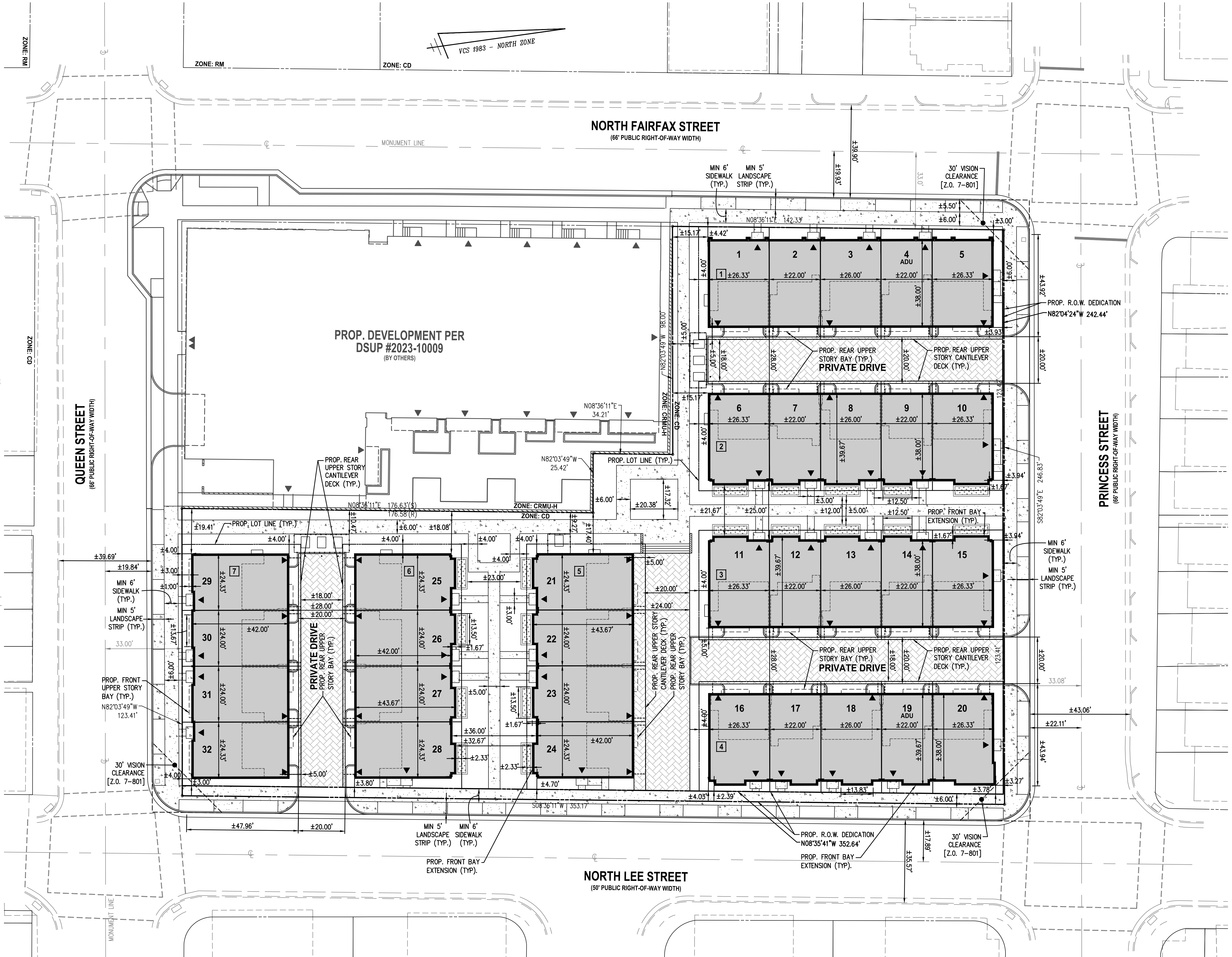
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LEGEND

PROPOSED	DESCRIPTION	EXISTING
EP	EDGE OF PAVEMENT	EP
MH	MANHOLE	MH
WV	WATER VALVE	WV
WM	WATER METER	WM
GM	GAS METER	GM
TCB	TRAFFIC CONTROL BOX	TCB
LP	LIGHT POLE	LP
LP/S	LIGHT POLE WITH SIGNALS	LP/S
	CURB & GUTTER CG-2	
	TRANSITION FROM CG-6 TO CG-6R	
	SANITARY SEWER S	
SL	SANITARY LATERAL	SL
• C.O.	CLEAN OUT	• C.O.
	STORM SEWER	
W	WATER MAIN	W
•	FIRE HYDRANT	•
E	PLUG	E
	OVERHEAD WIRES	
UE	UNDERGROUND ELECTRIC	UE
T	TELEPHONE	T
G	GAS MAIN	G
E	ELECTRICAL	E
	TRANSFORMER	
	HANDICAP RAMP (CG-12)	
	GUARDRAIL	
X	FENCE	X
	TRAFFIC FLOW	
	LIGHT	
	DOOR	
	TREES	
TP	TEST PIT	TP
LOD	LIMITS OF DISTURBANCE	
#	BUILDING NUMBER	

NOTES

1. INFORMATION & DETAILS SHOWN ON THESE PLANS ARE CONCEPTUAL AND SUBJECT TO CHANGE.
2. REFER TO ARCHITECTURE PLANS FOR DETAILS OF BUILDING DESIGN.



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F 301-634-8601
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SITE PLAN
333 N. Fairfax St.
City of Alexandria, VA

DATE	7/28/25
ISSUE/REVISION	
BAR CONCEPT SUBMISSION #1	
NO	

FILE NAME:
PLOT DATE: Jul. 25, 2025
DRAWING SCALE: 1" = 20'-0"

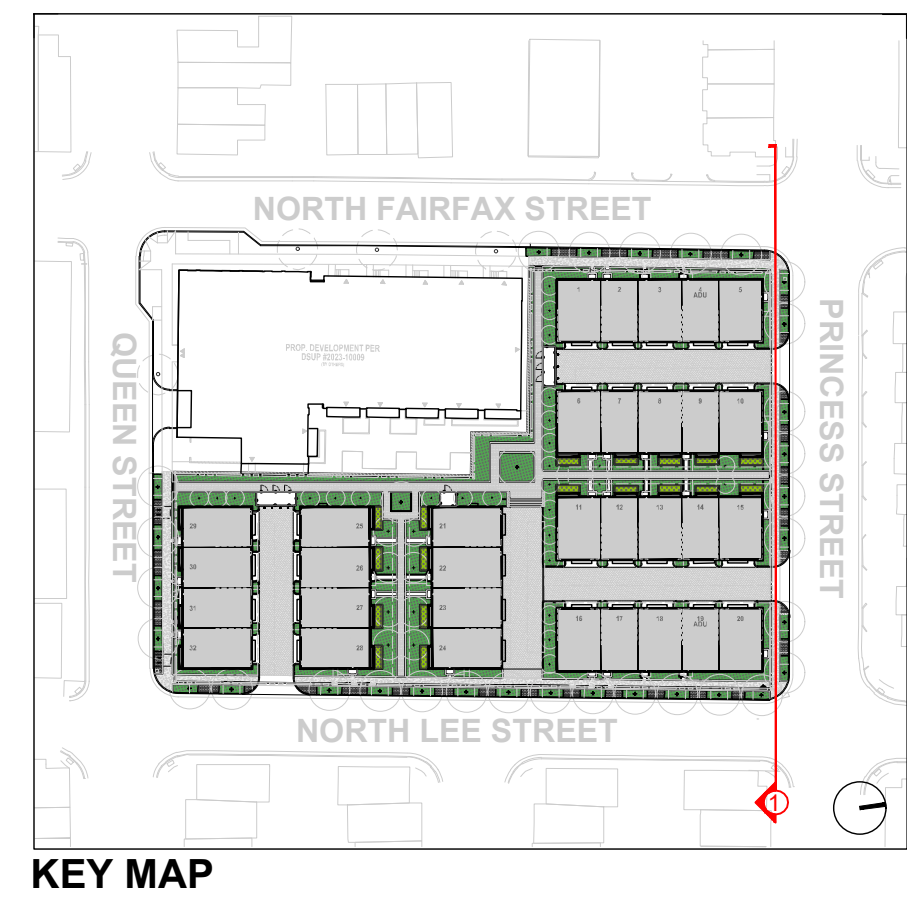
C-1.0

BAR CONCEPT SUBMISSION #1



SITE CONTEXT PLAN

333 N. Fairfax St.
City of Alexandria, VA



THE NEIGHBORHOODS OF
EYA.
life within walking distance®

333 N. Fairfax LLC

4800 Hampden Lane
Suite 300
Bethesda, MD 20814
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SITE SECTION

333 N. Fairfax St.
City of Alexandria, VA

DATE	7/28/25
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7/28/25

ISSUE/REVISION
ACCEPT SUBMISSION #1

BAR CONCEPT SUBMISSION #1

NO	
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FILE NAME:

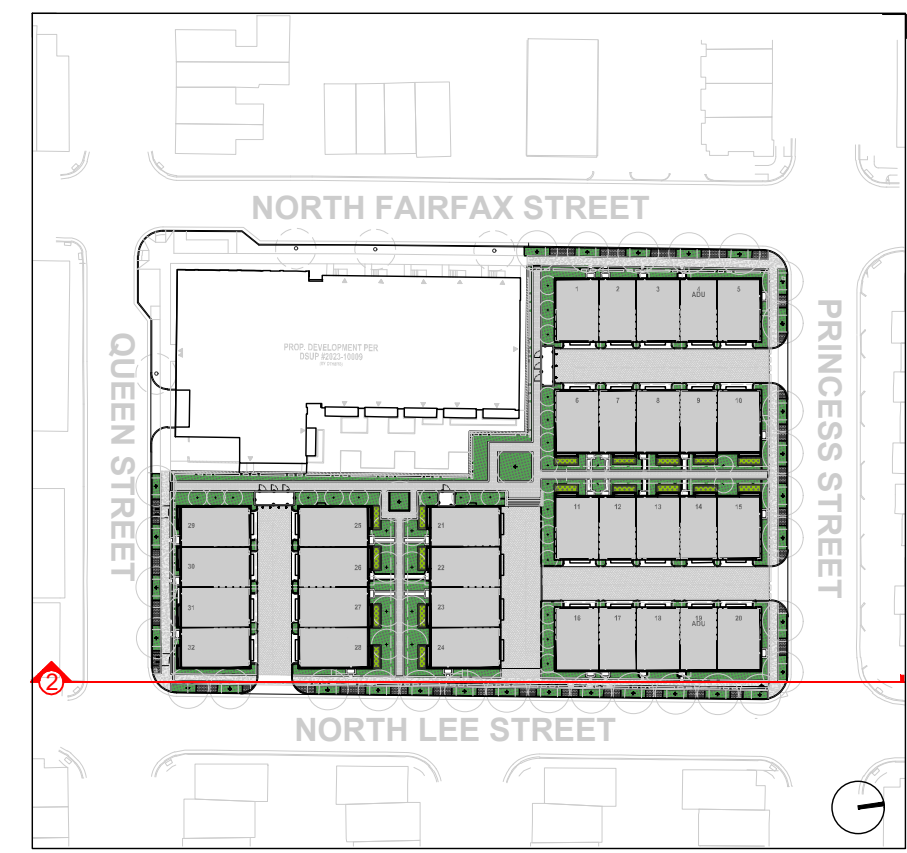
PLOT DATE:

DRAWING SCALE: 1' = 10"

C-4.0



BAR CONCEPT SUBMISSION #1



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SITE SECTION

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C-4.1

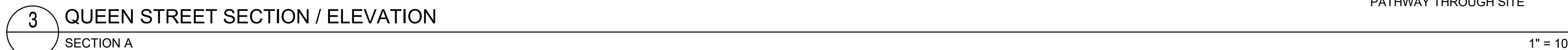


2 NORTH LEE STREET SECTION / ELEVATION

SECTION A



2 NORTH LEE STREET SECTION / ELEVATION
SECTION B



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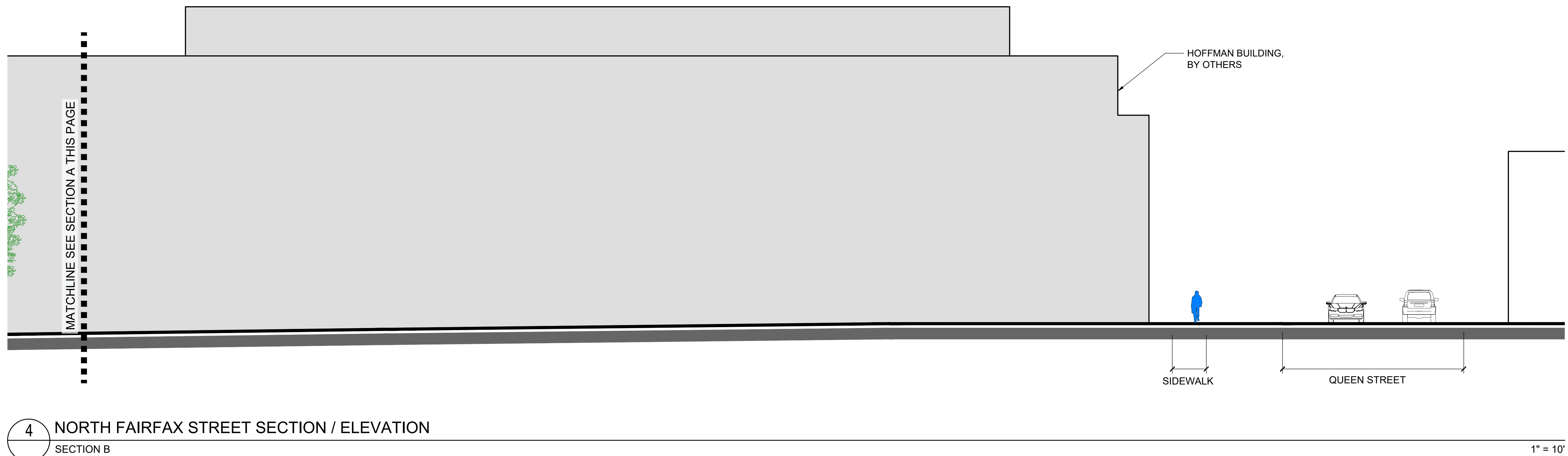
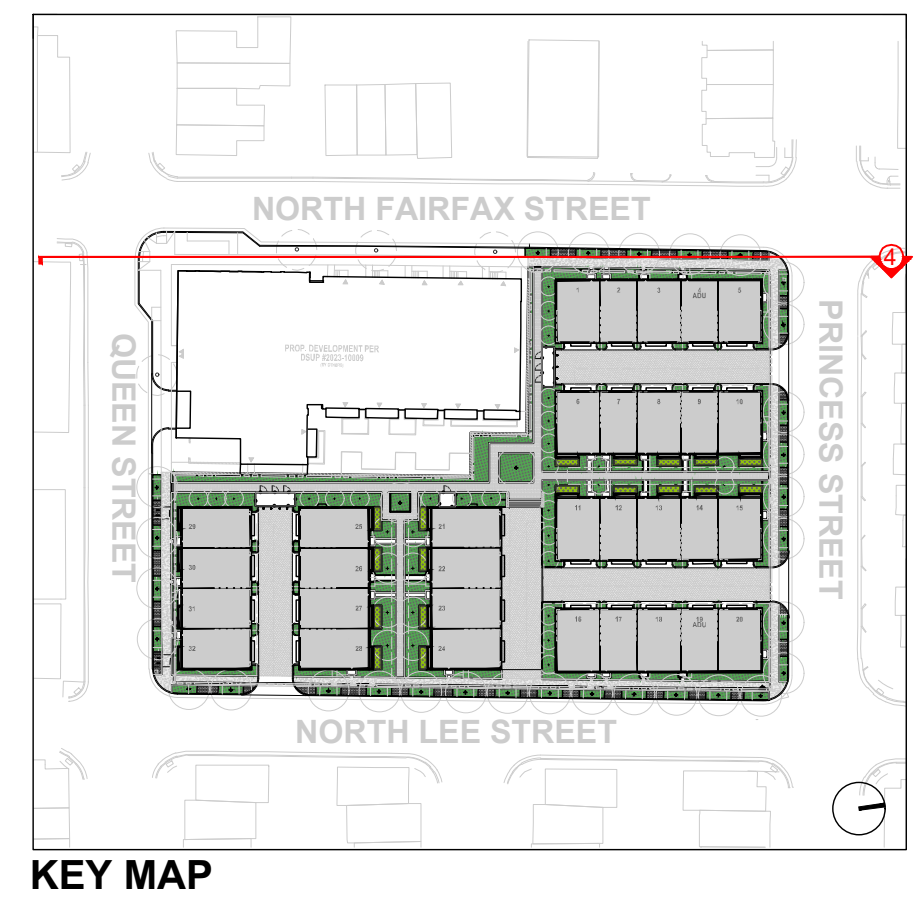
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[illegible]

C-4.2



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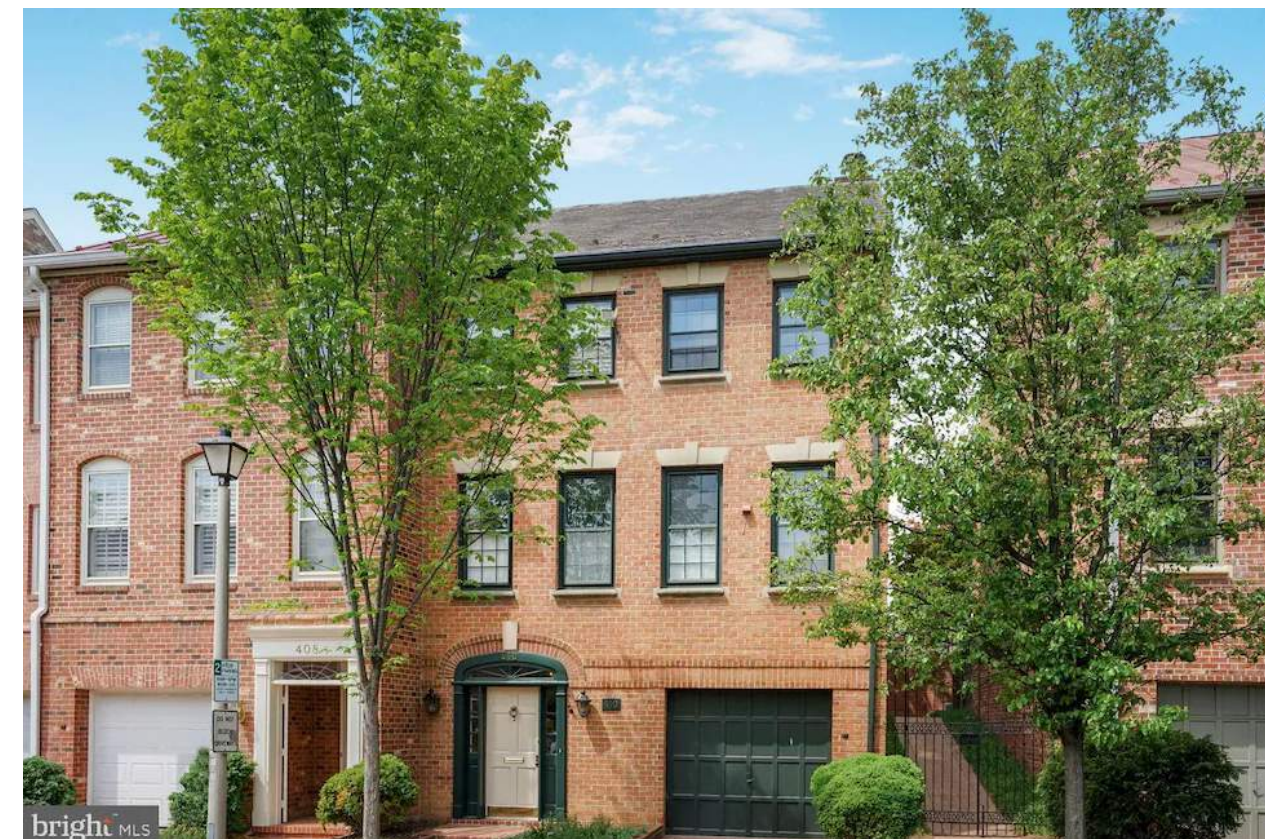
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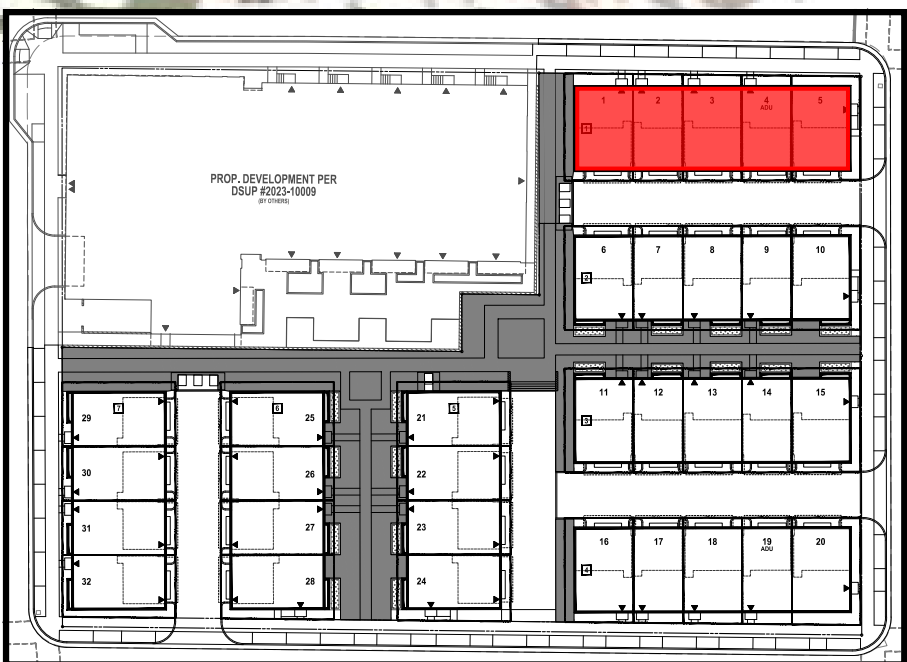
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BAR CONCEPT SUBMISSION #1

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C-4.3





BAR CONCEPT SUBMISSION #1





1 FRONT ELEVATION
Scale: 1/8"=1'



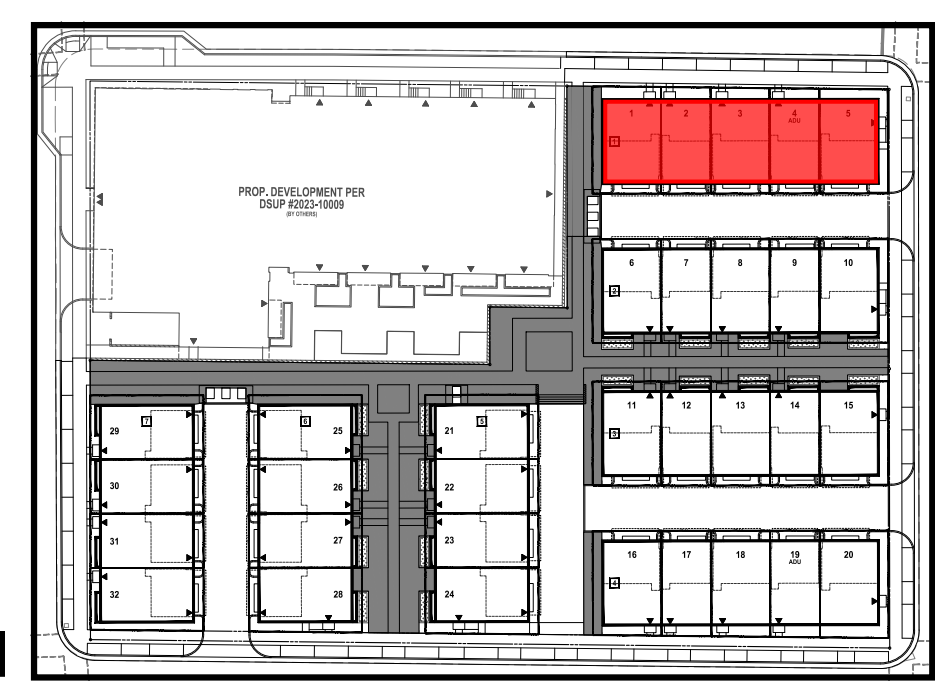
2 RIGHT SIDE ELEVATION
Scale: 1/8"=1'



3 REAR ELEVATION
Scale: 1/8"=1'



4 LEFT SIDE ELEVATION
Scale: 1/8"=1'



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BUILDING 1 - STYLE 2
ARCHITECTURAL ELEVATIONS

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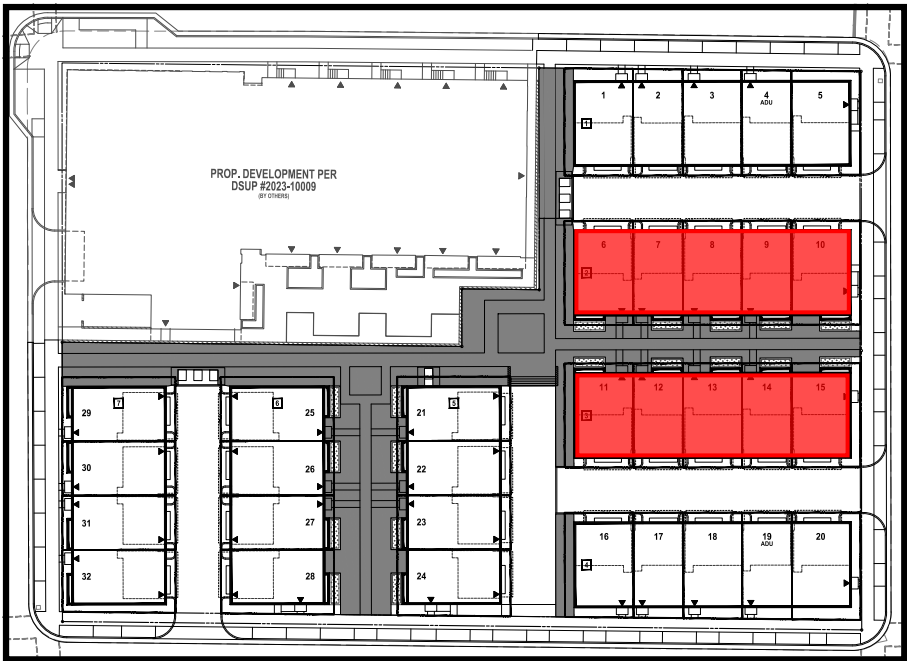
NO	ISSUE/REVISION	DATE
	BAR CONCEPT SUBMISSION #1	7/28/25

FILE NAME: 333-A-SHEET ELEVATION.dwg
PLOT DATE: 7.25.2025
DRAWING SCALE:

A-1.2

BAR CONCEPT SUBMISSION #1

28



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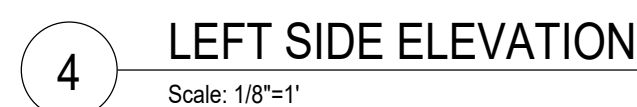
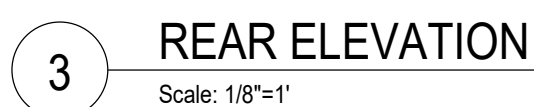
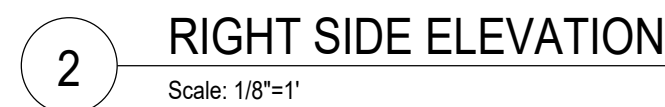
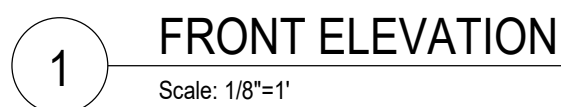
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BUILDING 2&3 - STYLE 3
PERSPECTIVE RENDERING
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City of Alexandria, VA

ISSUE/REVISION	DATE
NO	7/28/25
BAR CONCEPT SUBMISSION #1	
FILE NAME	333-A-SHEET ELEVATION.dwg
PLOT DATE:	7.25.2025
DRAWING SCALE:	

A-2.0

BAR CONCEPT SUBMISSION #1

[illegible]

BAR CONCEPT SUBMISSION #1

BUILDING 2&3 - STYLE 3
ARCHITECTURAL ELEVATIONS

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BUILDING 2&3 - STYLE 3

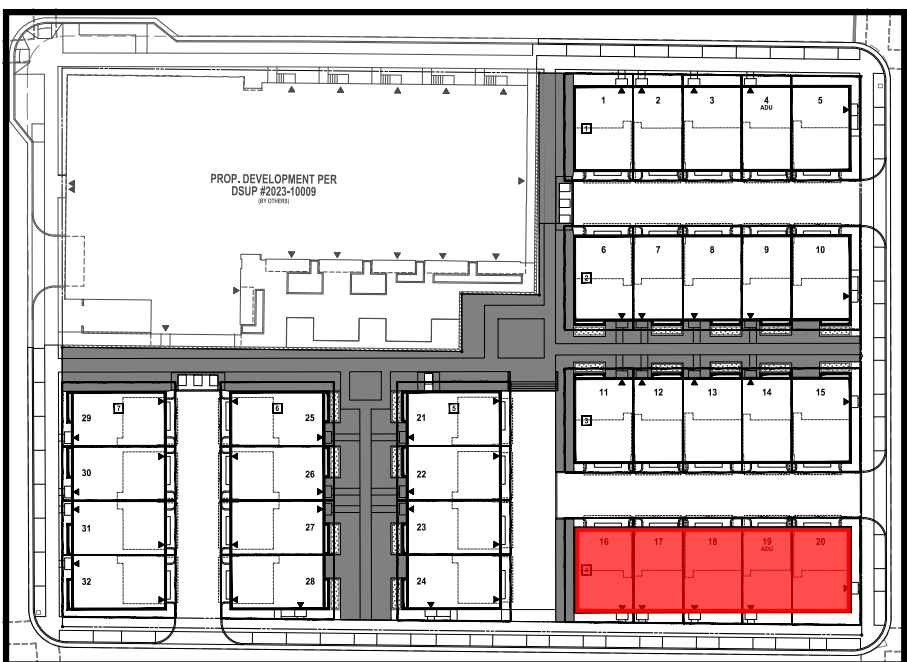
ARCHITECTURAL ELEVATIONS

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City of Alexandria, VA

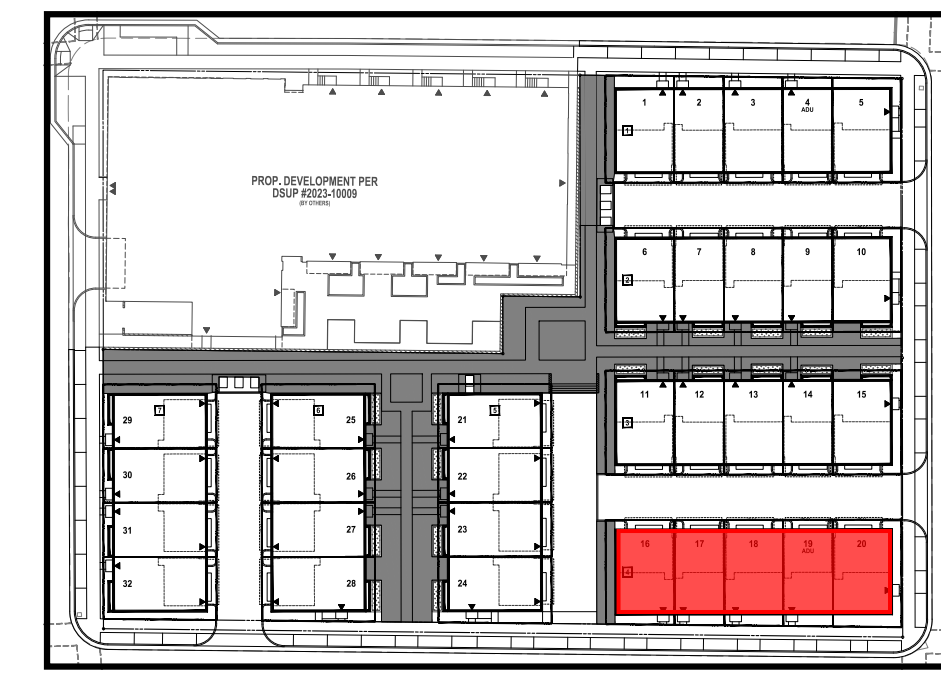
BAR CONCEPT SUBMISSION #1

[illegible]

A-2.2



32



KEY PLAN



1 FRONT ELEVATION
Scale: 1/8"=1'



2 RIGHT SIDE ELEVATION
Scale: 1/8"=1'



3 REAR ELEVATION
Scale: 1/8"=1'



4 LEFT SIDE ELEVATION
Scale: 1/8"=1'

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BUILDING 4 - STYLE 4
ARCHITECTURAL ELEVATIONS

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City of Alexandria, VA

[illegible]

A-3.1



1 FRONT ELEVATION
Scale: 1/8"=1'



2 RIGHT SIDE ELEVATION
Scale: 1/8"=1'

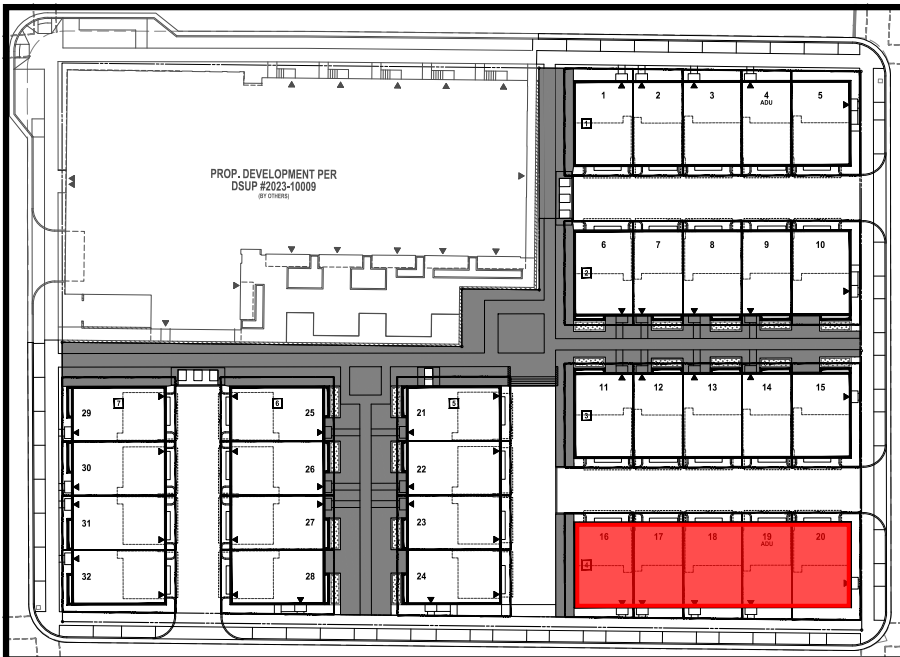


3 REAR ELEVATION
Scale: 1/8"=1'



4 LEFT SIDE ELEVATION
Scale: 1/8"=1'

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BUILDING 4 - STYLE 4
ARCHITECTURAL ELEVATIONS

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FILE NAME: 333-A-SHEET ELEVATION.dwg
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BUILDING 5&6 -STYLE 3

PERSPECTIVE RENDERING

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City of Alexandria, VA

NO.	BAR CONCEPT SUBMISSION #1	ISSUE/REVISION	DATE
			7/28/25

FILE NAME: 331-A-SHEET ELEVATION.dwg

PLOT DATE: 7.25.2025

DRAWING SCALE:

A-4.0





2 RIGHT SIDE ELEVATION
Scale: 1/8"=1'



4 LEFT SIDE ELEVATION
Scale: 1/8"=1'



BAR CONCEPT SUBMISSION #1



38

[illegible]



1 FRONT ELEVATION
Scale: 1/8"=1'



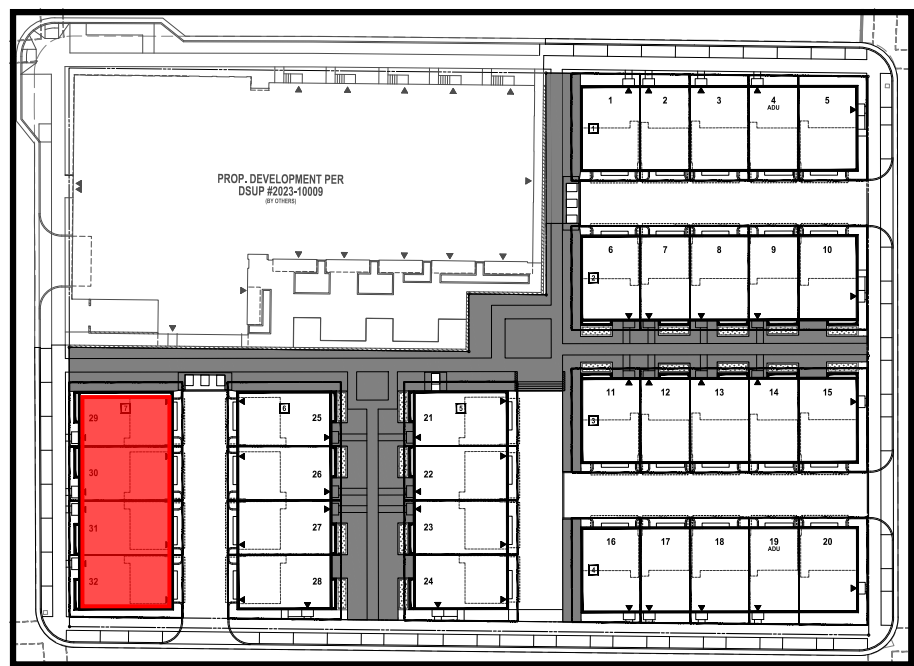
2 RIGHT SIDE ELEVATION
Scale: 1/8"=1'



3 REAR ELEVATION
Scale: 1/8"=1'



4 LEFT SIDE ELEVATION
Scale: 1/8"=1'



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BUILDING 7 - STYLE 1

ARCHITECTURAL ELEVATIONS

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FILE NAME: 333-A-SHEET ELEVATION.dwg
PLOT DATE: 7.25.2025
DRAWING SCALE:

A-5.2



[EXTERNAL]]Board of Architectural Review Public Hearing on 9/3/2025

From Daniel Bernstein <djbernstein@comcast.net>

Date Sat 8/16/2025 2:30 PM

To Ted Alberon <ted.alberon@alexandriava.gov>

You don't often get email from djbernstein@comcast.net. [Learn why this is important](#)

Hello: in reference to Sept. 3 Board of Architectural Review meeting I have comments on BAR #2025-00305 333 N. Fairfax St.

I am opposed to the overall design of these structures. If you take the time to walk or drive from the waterfront to N. Washington St. on Princess St. you will see that all the townhouse structures are basically individual traditional townhouse looking structures. This proposed industrial design is totally incompatible with our neighborhood in the Old and Historic District. When years ago you approved the removal of the large parking lot at N. Lee/Queen/N. Union St. it was replaced with traditional looking individual townhouses which matched the basic design of townhouses on Queen, Quay, Cameron and Princess St. This is the architectural look which is compatible with our neighborhood. You also approved this compatible design for Princess Walk in the 200 block of Princess St. as well as the more recent replacement of ARHA housing in the 300 block of Princess St. I am aware that areas outside the Historic District are building residential buildings with this ugly industrial design, but this has never been approved on Princess St. in the protected Historic District. Please reject this design.

Thank You,

Daniel and Christine Bernstein

121 Princess St.

Begin forwarded message:

From: Kendra Jacobs <Kendra.Jacobs@alexandriava.gov>