

***Master Plan Amendment #2025-00005***

***Rezoning #2025-00006***

***Special Use Permit #2025-00066***

***Encroachment #2025-00009***

***732 North Washington Street & 710 Madison Street***

***Office to Residential Conversion***

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Application	General Data	
<b>Request:</b> Public Hearing and consideration of a request for (A) an amendment to the Old Town North Small Area Plan Chapter of the Master Plan to change the Recommended Height District Limits Map to allow for 54 feet in building height; (B) a Rezoning from CD-X/Commercial Downtown Zone (Old Town North) to CRMU-X/Commercial Residential Mixed Use (Old Town North) zone; (C) Special Use Permits for a multi-unit dwelling use and a parking reduction; and (D) an Encroachment into the public right-of-way for upper floor balconies.	<b>Planning Commission Hearing:</b>	January 6, 2026
	<b>City Council Hearing:</b>	January 24, 2026
<b>Address:</b> 732 North Washington Street & 710 Madison Street	<b>Zone:</b>	<u>Existing:</u> CD-X/Commercial Downtown Zone (Old Town North) <u>Proposed:</u> CRMU-X/Commercial-Residential-Mixed-Use (Old Town North) zone
<b>Applicant:</b> 732-806 Development LLC represented by Kenneth Wire, attorney	<b>Small Area Plan:</b>	Old Town North
<b>Staff Recommendation:</b> APPROVAL		
<b>Staff Reviewers:</b> Tony LaColla, AICP, <a href="mailto:anthony.lacolla@alexandriava.gov">anthony.lacolla@alexandriava.gov</a> Sam Shelby, <a href="mailto:sam.shelby@alexandriava.gov">sam.shelby@alexandriava.gov</a>		

**PLANNING COMMISSION ACTION, JANUARY 6, 2026:** On a motion by Vice Chair Koenig, seconded by Commissioner Manor, the Planning Commission voted to recommend approval of MPA #2025-00005. The motion carried on a vote of 7 to 0.

On a second motion by Vice Chair Koenig, seconded by Commissioner Manor, the Planning Commission voted to recommend approval of REZ #2025-00006. The motion carried on a vote of 7 to 0.

On a final motion by Vice Chair Koenig, seconded by Commissioner Manor, the Planning Commission voted to recommend approval of SUP #2025-00066 and ENC #2025-00009. The motion carried on a vote of 7 to 0.

Reason:

The Planning Commission agreed with staff's analysis and recommendations.

Discussion:

Commissioner Manor clarified with staff the number of proposed units, how the off-street parking spaces would be configured, and what the proposed fifth floor renovations and rooftop improvements entailed.

Commissioner Brown highlighted that the adjacent lot across the alley behind the subject property is included in the site area and therefore can be used to calculate the maximum floor area ratio (FAR). He observed that this seemed to be unique and asked staff to clarify. Staff explained that the Zoning Ordinance allows this only when a tract of land is within a Coordinated Development District or one of Commercial Residential Mixed-Use (CRMU) zones. Commissioner Brown observed that, because of this, the adjacent lot could not be re-developed.

Commissioner Dube asked staff and the applicant to confirm the location of the dumpster enclosure and where loading and unloading would occur. Staff explained that the dumpster enclosure would be located behind one of the parking spaces across the alley from the building. Staff also explained that the proposed use would not be required to provide off-street loading spaces but that any use of the street or alley by future residents would require permission from the City.

Commissioner Ramirez spoke in support of the request. She explained that the office market has changed substantially in recent years and found that vacant office buildings create a general lack of foot traffic. Commissioner Ramirez was also supportive of the re-use of the existing building instead of demolition and redevelopment.

Chair McMahon also voiced support for the request, finding that it included modest proposed changes to the building itself and was a suitable site for conversion. She explained that Planning Commission, City staff, and the market would continue to evaluate the suitability of future conversion requests.

Vice Chair Koenig echoed Chair McMahon's support and observation about tracking office-to-residential conversions. He found that mixed-use development generally offers the most vibrancy and activity levels but that it was not possible at this site.

Speakers:

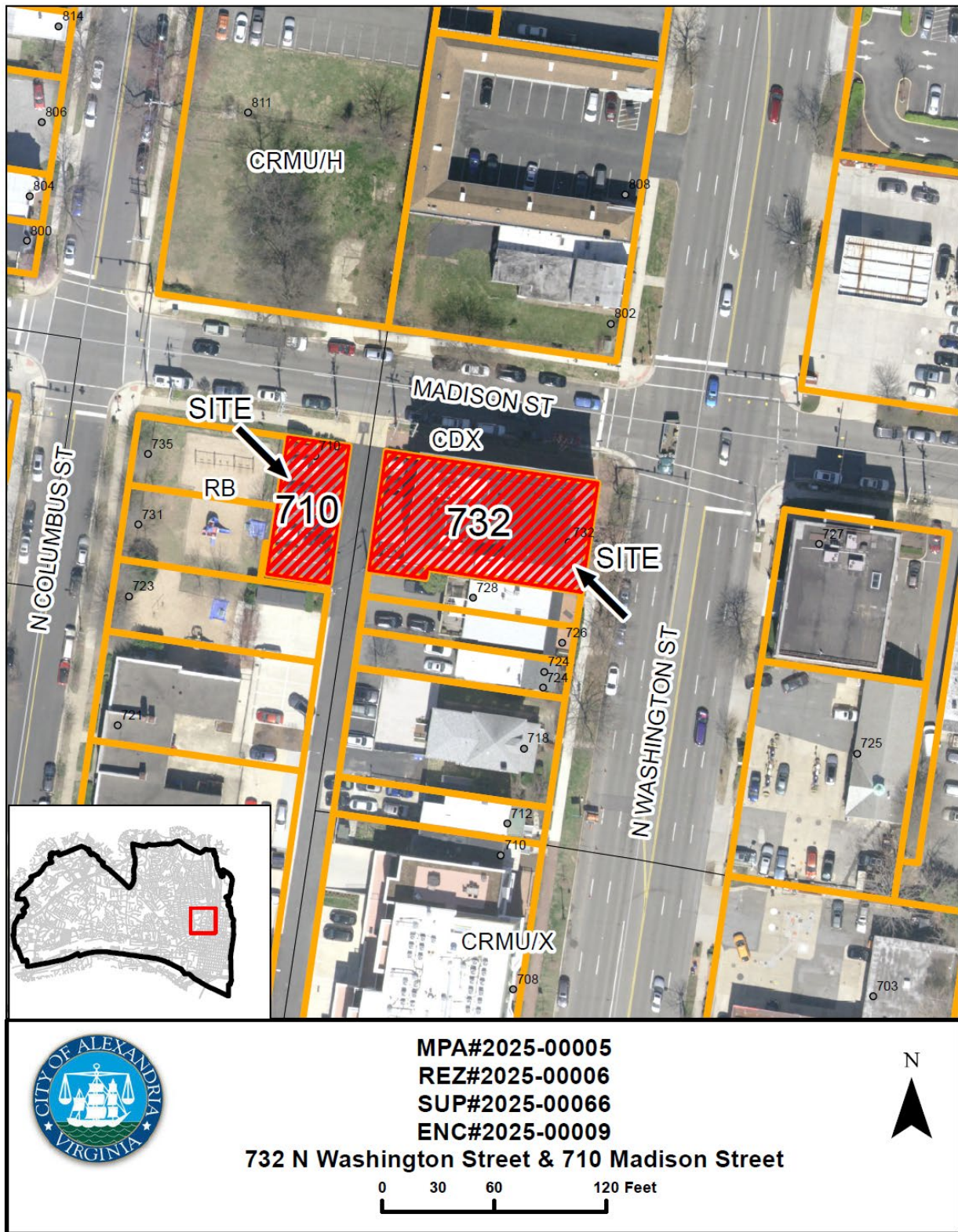
Melissa Kuennen, 525 Montgomery Street, disagree with staff and the applicant's conclusions that the conversion would be supported by the Old Town North Small Area Plan's (SAP) goals and objectives. She stated that the SAP encourages office retention. She also stated that conversions would decrease activity and walkability along North Washington Street.

Ken Wire, attorney representing the applicant, explained that the building has been functionally vacant for two years.

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He also clarified the parking configuration as requested by Commissioner Dube. He also cited a 2017 City Council policy that acknowledged issues with vacant office buildings and the benefits of office-to-residential conversions generally.

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## I. PROJECT SUMMARY

The applicant, 732-806 Development LLC, represented by Kenneth Wire, attorney, requests:

- (A) A **master plan amendment (MPA)** to the Old Town North Small Area Plan (SAP) chapter to change the Recommended Height District Limits Map to allow for 54 feet in building height;
- (B) a **rezoning from CD-X/Commercial Downtown Zone (Old Town North)** to **CRMU-X/Commercial Residential Mixed Use (Old Town North) Zone**;
- (C) **special use permits (SUP)** for a multi-unit dwelling use and a parking reduction; and
- (D) an **encroachment** into the public right-of-way for proposed upper floor balconies.

These approvals would allow an office-to-residential conversion of the existing building at 732 North Washington Street. The applicant proposes 20 dwelling units.

Staff **recommends approval** of all four requests as they meet all applicable criteria and are consistent with the City's planning goals and strategies for office-to-residential conversions as envisioned in the Old Town North Small Area Plan (SAP).

## II. SITE DESCRIPTION AND CONTEXT

The subject property contains two lots of record at 732 North Washington Street and 710 Madison Street. In total, the subject property contains 9,425 square feet of site area. The lot at 732 North Washington Street is located at the southwest corner of North Washington and Madison Streets. A five-story office building, constructed in 1963 occupies the site. The building has 24,326 square feet of floor area, which, based on the size of the subject property, results in a Floor Area Ratio (FAR) of 2.58. As measured to the top of the fifth-floor mansard roof, the building is 53.6 feet tall. No additions or significant alterations have been made to the building since its construction.

The building occupies almost the entire site but provides an 18.7-foot west side yard along a 20-foot-wide public alley. Figure one, below, shows the subject building.

The subject property is surrounded by a mix of uses including gas stations, offices, a school, and future multi-unit residential (see figure four, below). Due to its location on North Washington Street, the subject property is within the Old and Historic Alexandria District (OHAD). As such, any exterior changes will require review by the Board of Architectural Review.

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*Figure 1 – Subject property*



*Figure 2 – Surrounding buildings/uses*

### **III. BACKGROUND**

#### **EXISTING BUILDING**

The subject property's five-story building was developed pursuant to Site Plan #1963-00040. The approvals allowed a building with a maximum height of 55.67 feet, a 2.70 FAR, and a floor area of 25,400 square feet. At the time of approval, the property was zoned C-2/Commercial which allowed a maximum FAR and height of 3.0 and 77 feet, respectively.

#### **ZONING HISTORY**

In 1992, the subject property was rezoned to CD-X/Commercial Downtown Zone (Old Town North) as part of a comprehensive City-wide zoning update, and the C-2/Commercial zone was deleted from the Zoning Ordinance. The new CD-X zone allowed a maximum 1.5 FAR for nonresidential uses. Given that the building provides a 2.58 FAR, it became a noncomplying structure with the 1992 rezoning. Noncomplying structures may remain indefinitely subject to Zoning Ordinance section 12-100.

#### **NONCOMPLYING STRUCTURE LIMITATIONS**

Generally, any permitted use can occupy a noncomplying structure provided that it meets all applicable requirements. As an example, in the CD-X zone, residential buildings must provide side setbacks while nonresidential buildings are exempt from setback requirements. Given that the existing nonresidential building provides no setbacks, and the change in use would trigger the CD-X zone's residential requirements, a residential use would not be permitted without approved modifications from setback requirements.

Further, the CD-X zone permits a lower maximum FAR for residential uses (1.25) than it does nonresidential uses (1.5).

As a residential building, the subject property would be more out of compliance with maximum permitted FAR and would therefore not be permitted.

#### **IV. SMALL AREA PLAN**

The subject property is located within both Subarea One (Washington Street Gateway) and Four (Mixed Use Core) of the Old Town North Small Area Plan (SAP) chapter of the City's Master Plan. Both Subareas strongly encourage a mix of buildings and uses (residential, retail, and office) within their boundaries. North Saint Asaph Street, from Wythe to Montgomery is designated as one of the SAP's required retail or arts and cultural uses corridors.

The SAP does not identify the subject property as a potential redevelopment site nor does it provide specific rezoning recommendations. It acknowledges that office-to-residential conversions could occur in Old Town North given nationwide office-to-residential trends, current market conditions., and its inventory of aging office buildings. The SAP states that sites like the subject property would likely be conversion candidates given the age of the existing building and its smaller footprint.

#### **V. PROJECT DESCRIPTION AND REQUIREMENTS**

The applicant proposes converting the subject property's existing office building to a multi-unit residential use with 20 dwelling units. There would be no changes to the building's footprint nor increases to overall height or floor area.

The applicant proposes minor exterior improvements including façade refinements, balconies, rooftop access via stair penthouses, and an expansion to the fifth floor. Open space for residents would be provided on the roof. Several units would also have individual balconies. All exterior changes would require approval by the Board of Architectural Review. To accomplish the conversion, the applicant has requested a rezoning from CD-X to CRMU-X, SUPs for a multi-unit dwelling use and parking reduction, a master plan amendment to change the maximum permitted height from 50 to 55 feet, and an encroachment for the upper-level balconies.

##### **ZONING CONSTRAINTS**

As established in Section II, the existing CD-X zone would not permit residential use of the subject property due to two primary constraints: 1) 28-foot south and west side setbacks would be required and the building provides no setbacks on its south side and an 18.7-foot setback on its north side, and 2) the change in use would subject the property to the zone's lower residential FAR limit and would therefore increase the building's FAR noncompliance.

##### **PROPOSED ZONING**

The applicant requests a rezoning to CRMU-X. This zone does not require setbacks for residential uses. It also allows a higher FAR for residential uses than it does nonresidential. Because the proposed zone would not require side yards for the proposed residential use nor would it increase the building's FAR noncompliance, the proposal would free the subject property from the zoning



constraints established by the CD-X zone (and discussed in the previous section). Therefore, the proposal would be permitted with rezoning and SUP approval.

#### MASTER PLAN AMENDMENT

The subject building is an existing five-story structure measuring 53.6 feet tall. It was constructed prior to the adoption of the 50-foot height limit in the Old Town North Small Area Plan's (SAP) Recommended Height Limits Map and is therefore considered a noncomplying structure.

The applicant proposes a modest expansion to the building's fifth floor. The existing fifth floor does not have a full ceiling height across the full footprint of the floors below. Along the building's perimeter, about five feet from the edge of the roof, the fifth floor's roofline is sloped. This reduces the fifth floor's ceiling height around the full perimeter of the building. The proposed addition would fill in these areas with space that has full ceiling height and would result in a mansard roof profile.

Additions to noncomplying structures must comply with applicable zoning requirements, including the height limit established by the SAP's height map. To accomplish the proposed addition, the applicant has requested a Master Plan Amendment to change the height limit of the subject property only from 50 to 55 feet. Figure 3 shows the existing North Washington Street elevation and, though final design could be refined and will ultimately be subject to approval by the Board of Architectural review, figure 4 illustrates the proposed conceptual elevation.

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*Figure 3 – Existing North Washington Street elevation*

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*Figure 4 – Preliminary North Washington Street elevation*

### PARKING

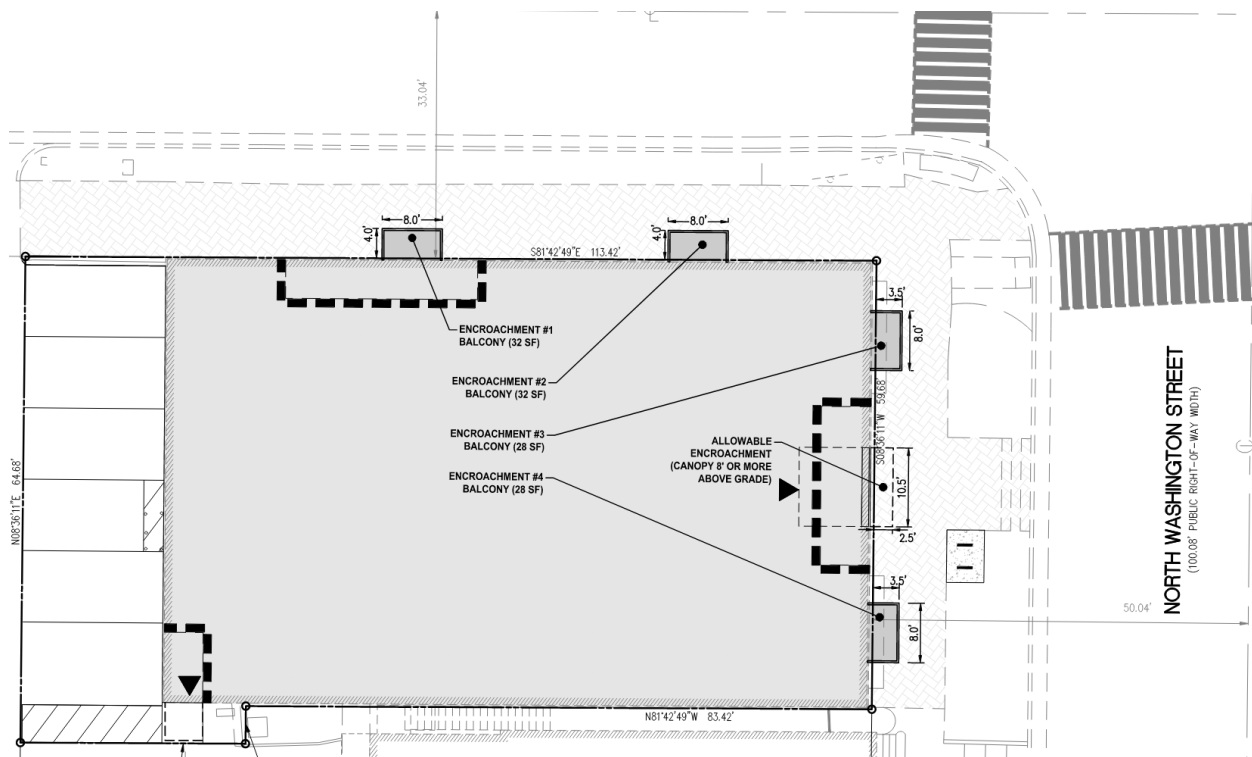
The Zoning Ordinance would require a total of 20 off-street parking spaces for the proposed residential use. Up to 15 spaces (75 percent of the total requirement) of these spaces may be compact. The subject property would provide 23 total spaces. Of these, 5 would be standard and the remaining would be compact. Further, 8 spaces would be in a tandem arrangement and could not be counted toward the off-street parking requirement. Given the foregoing, the subject property can provide a maximum of 15 total off-street parking spaces. As such, the applicant requests a five-space parking reduction.

### HISTORIC PRESERVATION

As stated in Section I, the subject property is located within the Old and Historic Alexandria District. Any exterior alterations to the building will require review and approval by the Board of Architectural Review (BAR).

### ENCROACHMENTS

The applicant requests encroachment approval for second to fourth floor balconies located in the North Washington and Madison Street rights-of-way (ROW). The balconies would be at least 11 feet above the sidewalk grade.



*Figure 5 – Proposed encroachments*



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### ZONING TABLE

The table below summarizes all zoning regulations applicable to the request.

	<i>Existing</i>	<i>Permitted/Required (CD-X)</i>	<i>Permitted/Required (CRMU-X)</i>	<i>Proposed</i>
<b>FAR</b>	2.58 (24,326 sq. ft.)	<u>Nonresidential:</u> 1.5 (14,137 sq. ft.) <u>Residential:</u> 1.25 (11,781 sq. ft.)	<u>Nonresidential:</u> 1.5 (14,137 sq. ft.) <u>Residential:</u> 2.5 (23,562 sq. ft.)	2.4 (23,144 sq. ft.)
<b>Height</b>	53.6 Ft.	50 Ft.	50 Ft.	No change
<b>Open Space</b>	None	<u>Nonresidential:</u> None <u>Residential:</u> 35% (2,422 sq. ft.)	<u>Nonresidential:</u> None <u>Residential:</u> 25% (1,729 sq. ft.)	25% (1,730 sq. ft.)
<b>Front Yard (N. Washington St.)</b>	0 Ft.	<u>Nonresidential:</u> None <u>Residential:</u> 0 Ft. (minimum) 1 Ft. (maximum)	<u>Nonresidential/ residential:</u> None	No changes
<b>Front Yard (Madison St.)</b>	0 Ft.			
<b>Side Yard (South)</b>	0 Ft.			
<b>Side Yard (West)</b>	18.7 Ft.			
<b>Parking</b>	20 spaces (9 tandem, 18 compact)	<u>Nonresidential:</u> 7 spaces (minimum) 36 spaces (maximum) <u>Residential:</u> 20 spaces (up to 15 compact)		23 spaces (8 tandem, 18 compact)

## VI. STAFF ANALYSIS

Staff recommends approval of the applicant's requests. The proposed adaptive reuse proposal would advance SAP goals for office-to-residential conversions and would eliminate the building's FAR noncompliance. The project would likely have lower impacts than the existing office use and would maintain neighborhood character through preservation of the existing building envelope. Further analysis follows.

### REZONING CONSIDERATIONS

Staff recommends approval of the applicant's rezoning request as it would be consistent with SAP.

#### *Appropriateness of CRMU-X zone*

The proposed CRMU-X zone would uphold the SAP's land use goals for Subareas 1 and 4. These subareas, Old Town North's "mixed-use core," already contains numerous properties zoned CRMU-X and CRMU-H. The SAP establishes cohesive land use goals for these subareas despite mixed zoning designations. Both the existing CD-X zone and CRMU-X zone exist within the subareas which indicates that both zones would be suitable for achieving and maintaining the desired mixed-use character envisioned by the SAP. Further, the redevelopment site across Madison Street shares the CRMU-X zoning designation.

#### *Noncomplying Status*

As previously stated, unlike the CD-X zone, the CRMU-X zone doesn't require setbacks for residential uses. Given this, the zone would be appropriate for the subject property given that the existing building provides deficient setbacks. Under the existing CD-X zone, modifications would be required from setback requirements to allow a residential use.

Given the applicant's proposed renovations to the building, which includes square footage eligible for exclusion from floor area, the resulting FAR would be 2.4. That combined with the proposed rezoning to the CRMU-X zone, which allows a 2.5 FAR would eliminate the existing FAR noncompliance. As such, the proposed rezoning would eliminate the building's FAR noncompliance.

#### *Mix of Uses*

The subject property is surrounded by a range of uses. The SAP states that "...to provide a balance of residents and employees, approximately two to three times more residential than office square footage is necessary." The proposal would add residential square footage that would better balance the mix of office and residential uses in the surrounding neighborhood.

#### *Office-to-Residential Conversions*

As stated in Section I, the SAP predicts several office-to-residential conversions within Old Town North. The existing building has characteristics that the SAP envisions would make it an appropriate conversion candidate: age, condition, and lack of amenities. The SAP also encourages "...innovative housing solutions such as... building conversions." Given that the office-to-residential conversion of the subject property would not disrupt the mix of uses in the area, it has characteristics that make it an appropriate conversion candidate, and that such conversions are encouraged to increase housing supply, the proposed conversion would advance the SAP's goals.

### MASTER PLAN AMENDMENT CONSIDERATIONS

Staff supports the requested master plan amendment finding the proposal to be consistent with the Old Town North SAP. The amendment is technical in nature: it would formalize the building's existing height on the SAP's Height District Limits Map rather than authorizing a meaningful increase to the building's height. The existing building and proposed addition would be compatible with several surrounding buildings along North Washington Street in terms of height. The proposed addition fills in existing areas of the building within the existing building envelope so the visual height impact would be minimal. The proposed amendment would allow for interior improvements without altering the building's established scale or visual compatibility with the surrounding neighborhood.

Staff has recommended increasing the height map limit from 50 to 55 feet to account for any possible variation in grade and height measurements as the building plans are finalized. To ensure that the overall height does not increase, staff has included condition #13, below which stipulates that the final height shall not exceed the existing height of the building.

### SUP CONSIDERATIONS

#### *Use Intensity and Anticipated Impacts*

Staff recommends approval of the SUP request for a multi-unit dwelling use because all required criteria would be met. If the building were fully occupied with office spaces, there would likely be more building users than would be expected with the building fully occupied by residential uses. Given this, staff expects no undue impact to the surrounding neighborhood.

Future residents would be present on nights and weekends when offices are typically closed but this would be an asset to the neighborhood and would increase consistency with the SAP as it would increase pedestrian activity levels in the SAP's "mixed-use core."

#### *Building Size and Function*

The proposal would be compatible with neighborhood character as the existing building would be preserved at its current size, height, and massing. The applicant proposes façade refinements that would make the building design more compatible with the newer and newly-renovated buildings that surround it. The proposed rooftop improvements would likely be minimally visible from the street level and therefore would not impact visual compatibility.

The building would provide rooftop open space and balconies for residents' use. These areas would serve as functional and usable outdoor space, ensuring that residents would have access to fresh air and sunlight. The large balcony on the west side of the building would provide shared outdoor recreational space.

Staff has also included Condition #2, which would ensure that the applicant reduces impacts from the noise of the surrounding neighborhood on future residents.

### *Parking reduction*

Staff recommends approval of the applicant's request to reduce the parking requirement from 20 to 15 spaces.

While the site currently provides 20 spaces and the applicant proposes three additional spaces, eight of the spaces are configured in a tandem arrangement that cannot count toward the parking requirement. Despite this, the tandem spaces would work well for units with more than one vehicle.

Staff finds that providing the required spaces on site would be infeasible given that the existing building occupies the entire site. The building would have to be demolished to meet the off-street parking requirement.

Staff further finds that the parking reduction would not negatively impact the surrounding neighborhood given the site's transit-rich, walkable location. The property is a half mile from the Braddock Road Metro station and has multiple bus stops within a half block along both Madison and North Washington Streets. Multiple grocery stores and several restaurants are located within a three-block radius which would enable residents to commute and accomplish daily errands without relying on a personal vehicle.

### ENCROACHMENT CONSIDERATIONS

Staff recommends approval of the encroachment request for the proposed balconies. They upper-would provide modest outdoor space for residents and would be positioned well above street level, ensuring no interference with existing sidewalk usability or building access. Staff has included standard encroachment conditions to ensure ongoing compliance with City Code requirements while preserving the City's flexibility for future right-of-way improvements if needed.

### OUTREACH

The applicant held a community meeting on December 15, 2025 regarding the proposal and another of the applicant's projects. No significant concerns were raised and staff has not received community comments or concerns to date.

## **VII. VOLUNTARY HOUSING CONTRIBUTION**

Staff found that the [2020/21 Housing Contributions Policy Update's](#) commercial to residential conversion guidance as the appropriate approach to determining the voluntary affordable housing contribution for this project. Based on the 2025 commercial to residential conversion rate of \$1.83 per net square foot, staff estimate a voluntary monetary contribution of \$41,395.

To achieve zoning compliance, secure on-site units, and estimate a voluntary monetary contribution, staff analyzed the following policy approaches: 1) applying the 2020/21 Commercial to Residential Conversion voluntary monetary contribution policy (the recommended approach) and 2) applying the 2020/21 Rezoning Housing Contribution Policy Update. The Rezoning policy only applies for cases where the SAP envisions a lower FAR than what's proposed. Given that the Old Town North SAP doesn't establish specific FAR nor identify the subject property as redevelopment site, the Rezoning policy would not be appropriate.



Staff has included Condition #6 which recommends that the voluntary contribution be made prior to City issuance of the certificate of occupancy.

## VIII. RECOMMENDED CONDITIONS

Staff recommends **approval** subject to compliance with all applicable codes and ordinances and the following conditions:

1. Applicant will coordinate with a noise mitigation consultant to submit a noise study identifying the noise levels that residents will be exposed to initially and 10 years into the future per the Noise Guidance Book used by the Department of Housing and Urban Development prior to being occupied. (T&ES)
2. If the noise study identified noise impacted areas, applicant will conduct a building shell analysis identifying ways to minimize noise and vibration exposure to future residents. Applicant shall submit the building shell analysis and install upgrade recommendations identified in the building shell analysis prior to occupancy. (T&ES)
3. Applicant shall submit the noise study and building shell analysis to T&ES Environmental Quality team for review and approval prior to approval with Planning Commission and City Council. (T&ES)
4. No exterior loudspeakers are permitted on the roof deck. (T&ES)
5. Site shall comply with the City's Noise Control Code, Title 11, Ch. 5, which sets the maximum permissible noise level as measured at the property line, which includes all rooftop HVAC and other mechanical equipment shall comply with the City noise ordinance by equipment design, location, or with noise mitigating devices (e.g., silencers, acoustic plenums, louvers, or enclosures). (T&ES)
6. Prior to the issuance of the Certificate of Occupancy, contribute \$41,395 to the City's Housing Trust Fund. Make out all payments to the City of Alexandria and submit them to the Office of Housing with a cover letter to include the project name, case number, and explanation of the contribution amount, if phased. (P&Z)
7. The owner shall obtain and maintain an encroachment permit and policy of general liability insurance in compliance with the permit requirements in § 5-2-29(a)(3). (T&ES)
8. Neither the City nor any private utility company will be held responsible for damage to the approved encroachments arising from the repair, maintenance, or replacement of any utilities that may exist within the area of the approved encroachments. (T&ES)
9. If the City in the future needs the encroachment area, then the applicant shall remove any structure that encroached into the public right-of-way within 60 days upon written notification. If a longer period of time for removal is required, written permission of the City must be obtained. (T&ES)

10. The applicant is responsible for replacement and repairs to adjacent public right-of-way including damages due to construction or maintenance of the encroaching elements. (T&ES)
11. Balconies located within the encroachment areas shall be at least 8 feet above sidewalk grade. (P&Z)
12. Within the encroachment areas identified in the metes and bounds exhibit, the applicant may not erect any additional equipment or structures or enlarge or intensify any existing equipment or structures shall without prior approval of the Directors of P&Z and T&ES. No administrative changes may be made after approval of the encroachment ordinance. (P&Z)
13. No alterations or additions except those permitted by Zoning Ordinance section 6-403 shall exceed the height of the existing building. (P&Z)

STAFF: Tony LaColla, AICP, Division Chief  
Sam Shelby, Principal Planner  
Department of Planning and Zoning

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Staff Note: In accordance with section 11-506(c) of the zoning ordinance, construction or operation shall be commenced and diligently and substantially pursued within 18 months of the date of granting of a special use permit by City Council or the special use permit shall become void.

## IX. CITY DEPARTMENT COMMENTS

Legend: C - code requirement R - recommendation S - suggestion F - finding

### Transportation & Environmental Services:

- F-1 The Site is located within the [Enhanced Transit Area](#). It is also within Parking District #3. (Transportation Planning)
- F-2 The Site is subject to the [City's Residential Parking Permit Policy \(RPP\) for New Development](#). (Transportation Planning)
- F-3 As this site does not require a site plan, it is therefore not subject to the [City's TMP Policy](#). (Transportation Planning)
- F-4 With a conversion from office space to residential, the requirements for noise mitigation will change to identify potential needs to upgrade the windows or doors to reduce noise to appropriate levels
- R-1 Provide short-term and long-term bicycle parking pursuant to the City's Guidelines. Given the location – which experiences high volumes of bikes, peds, vehicles, etc., the preference is for the short-term spaces specifically to be located in a well-lit area (preferably covered from the elements by way of a canopy, awning, etc). (Transportation Engineering/Transportation Planning)
- R-2 Provide the parking study to determine if residents of this new development are eligible for RPP. See Finding #3 above. (Transportation Planning)
- R-3 Given the compactness of the asphalt [parking] surface, wheel-stops or similar are strongly encouraged for all parking stalls to prevent vehicles from striking the building. (Transportation Planning)
- R-4 Applicant will coordinate with a noise mitigation consultant to submit a noise study identifying the noise levels that residents will be exposed to initially and 10 years into the future per the Noise Guidance Book used by the Department of Housing and Urban Development prior to being occupied. (OEQ)
- R-5 If the noise study identified noise impacted areas, applicant will conduct a building shell analysis identifying ways to minimize noise and vibration exposure to future residents. Applicant shall submit the building shell analysis and install upgrade recommendations identified in the building shell analysis prior to occupancy. (OEQ)
- R-6 Applicant shall submit the noise study and building shell analysis to T&ES Environmental Quality team for review and approval prior to approval with Planning Commission and City Council. (OEQ)
- R-7 No exterior loudspeakers are permitted on the roof deck. (OEQ)
- R-8 Site shall comply with the City's Noise Control Code, Title 11, Ch. 5, which sets the maximum permissible noise level as measured at the property line, which includes all rooftop HVAC and other mechanical equipment shall comply with the City noise ordinance by equipment design, location, or with noise mitigating devices (e.g., silencers, acoustic plenums, louvers, or enclosures). (OEQ)

- C-1 Provide ‘Drainage Certification’ and ‘Disturbed Area Certification’ in the site plan from the Memoranda to Industry 02-08 dated April 28, 2008. Grading Plan requirements and waiver provisions, are available at the following web address. (SWM)

<http://www.alexandriava.gov/uploadedFiles/tes/info/gradingPlanRequirements.pdf>

Disturbed Area Certification:

*“I hereby certify that the limits of disturbance associated with this project represent a total land disturbance of less than 2,500 square feet. I further certify that no construction work, material storage, dumpster placement, construction access or disturbance of any other kind will take place beyond the limits of disturbance as depicted. I acknowledge that should this project result in land disturbance equal to or greater than 2,500 square feet, the City will issue a stop work order and work on the project will not be allowed to resume until the revision is submitted and approved by the City of Alexandria, Department of Transportation and Environmental Services.”*

Drainage Certification:

*“I hereby certify that the existing and proposed drainage patterns associated with this project are as depicted herein, that construction of this project will not create a nuisance to adjacent or downstream properties either public or private and that any existing drainage problems on adjacent or downstream properties either public or private will not be exacerbated by construction of this project. I acknowledge that should this project result in the creation of any nuisance, or exacerbation if any existing drainage problem, the City will issue a stop work order and work on this project will not be allowed to resume until the revision is submitted and approved by the City of Alexandria, Department of Transportation and Environmental Services.”*

- C-2 Should greater than 2,500 square feet of land be disturbed, the stormwater quality and quantity requirements of Article XIII, Environmental Management Ordinance, must be fulfilled. (SWM)
- C-3 Comply with the City’s Noise Control Code, Title 11, Ch. 5, which sets the maximum permissible noise level as measured at the property line, which includes all rooftop HVAC and other mechanical equipment shall comply with the City noise ordinance by equipment design, location, or with noise mitigating devices (e.g., silencers, acoustic plenums, louvers, or enclosures).

Code Enforcement:

- F-1 A building permit is required.

Fire:

No comments or concerns.

Recreation, Parks, and Cultural Activities:

- R-1 The conversion of office space to residential units is likely to increase demand on existing RPCA facilities and open spaces near the proposed site. Collaborate with City staff to mitigate the additional pressures associated with higher usership of amenities, services, and



maintenance of open spaces. Contributions to support the maintenance or operations of the Charles Houston Recreation Center should be considered. Additionally, the provision of on-site amenities, such as dog parks, children's playgrounds, and similar facilities, is encouraged to enhance the usability and functionality of the development area.

- R-2 Collaborate with RPCA to identify comparable amenities or facilities that can be provided in lieu of green space, provided that such amenities or facilities meet or exceed the beneficial purposes of the original green areas.
- a. Open space requirements provide allowance for the partial reduction in the minimum 25 percent of open space given that "a portion of the space which would otherwise be required as green area may be met by comparable amenities and/or facilities provided in lieu thereof if such amenities or facilities meet or exceed the beneficial purposes which such green areas would accomplish. A determination by the director or by council in the case of a special use permit shall be made in each case as to whether the open space provided, in addition to meeting the technical definition of open space, is functional and usable or includes comparable amenities" (5-408).

Office of Historic Alexandria:

No comments.

**Master Plan Amendment Resolution**

RESOLUTION NO. **MPA 2025-00005**

WHEREAS, under the Provisions of Section 9.05 of the City Charter, the Planning Commission may adopt amendments to the Master Plan of the City of Alexandria and submit to the City Council such revisions in said plans as changing conditions may make necessary; and

WHEREAS, the proposed amendment will amend the **Old Town North Small Area Plan** chapter of the 1992 Master Plan;

WHEREAS, the Department of Planning and Zoning has analyzed the proposed revisions and presented its recommendations to the Planning Commission; and

WHEREAS, a duly advertised public hearing on the proposed amendment was held on **January 6, 2026** with all public testimony and written comment considered; and

WHEREAS, the Planning Commission finds that:

1. The proposed amendment is necessary and desirable to guide and accomplish the coordinated, adjusted and harmonious development of the **Old Town North Small Area Plan** section of the City; and
2. The proposed amendment is generally consistent with the overall goals and objectives of the 1992 Master Plan and with the specific goals and objectives set forth in the **Old Town North Small Area Plan** section of the 1992 Master Plan; and
3. The proposed amendment shows the Planning Commission's long-range recommendations for the general development of the **Old Town North Small Area Plan**; and
4. Based on the foregoing findings and all other facts and circumstances of which the Planning Commission may properly take notice in making and adopting a master plan for the City of Alexandria, adoption of the amendment to the **Old Town North Small Area Plan** chapter of 1992 Master Plan will, in accordance with present and probably future needs and resources, best promote the health, safety, morals, order, convenience, prosperity and general welfare of the residents of the City;

NOW, THEREFORE, BE IT RESOLVED by the Planning Commission of the City of Alexandria that:

1. The attached amendment to the **Old Town North Small Area Plan** is hereby adopted in its entirety amending the Old Town North Small Area Plan chapter of the 1992 Master Plan

of the City of Alexandria, Virginia in accordance with Section 9.05 of the Charter of the City of Alexandria, Virginia, to:

- Amend Figure 2.14: Recommended Height District Limits, to change the height limit for the property from 50 feet to 55 feet (page 43).
2. This resolution shall be signed by the Chair of the Planning Commission and attested by its secretary, and a true copy of this resolution forwarded and certified to the City Council.

ADOPTED the 6<sup>th</sup> day of January, 2026.



Chair, Alexandria Planning Commission

ATTEST:



Paul Stoddard, Secretary

## X. CITY DEPARTMENT COMMENTS

Figure 2.14 Recommended Height Limits, Existing (pg. 43)

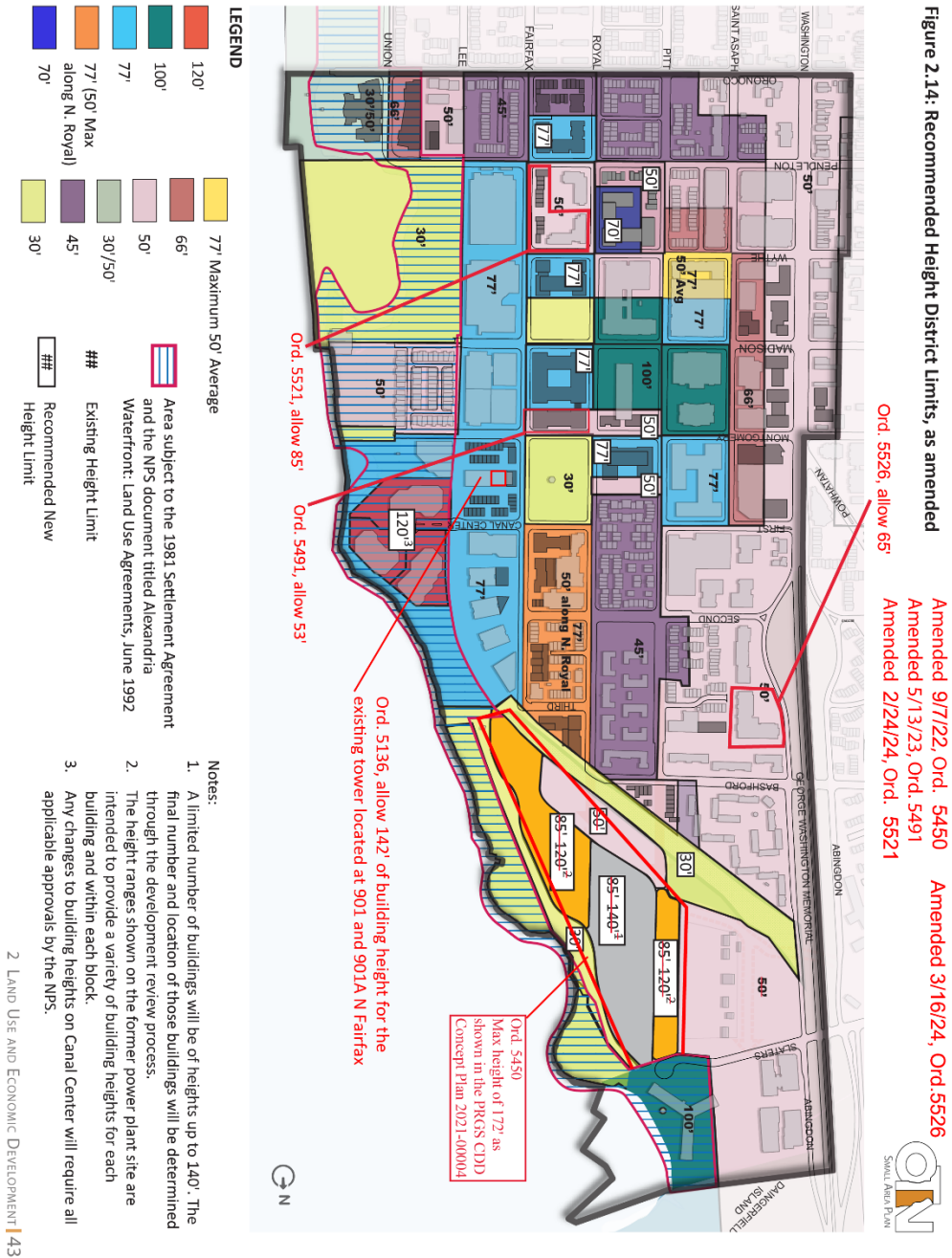
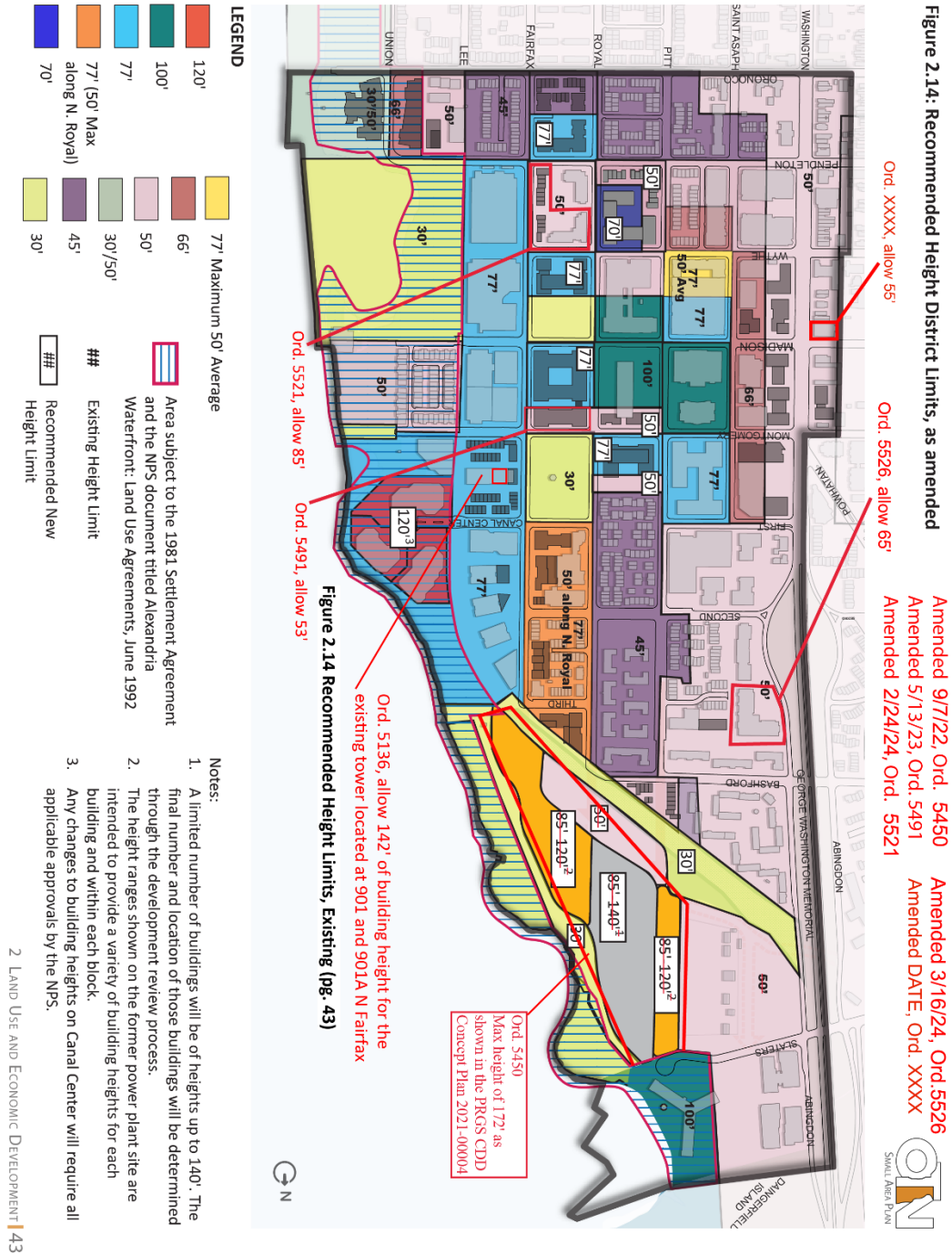


Figure 2.14 Recommended Height Limits, Proposed (pg. 43)





## APPLICATION

☒ **Master Plan Amendment MPA#** \_\_\_\_\_  
☒ **Zoning Map Amendment REZ#** \_\_\_\_\_

**PROPERTY LOCATION:** 732 N Washington Street, Alexandria VA 22066

### APPLICANT

Name: 732-806 Development LLC

Address: \_\_\_\_\_

### PROPERTY OWNER:

Name: American Statistical Association

Address: \_\_\_\_\_

### Interest in property:

☐ Owner ☒ Contract Purchaser  
☐ Developer ☐ Lessee ☐ Other \_\_\_\_\_

If property owner or applicant is being represented by an authorized agent such as an attorney, a realtor, or other person for which there is some form of compensation, does this agent or the business in which they are employed have a business license to operate in Alexandria, VA:

☐ Yes: If yes, provide proof of current City business license.  
☐ No: If no, said agent shall obtain a business license prior to filing application.

**THE UNDERSIGNED** certifies that the information supplied for this application is complete and accurate, and, pursuant to Section 11-301B of the Zoning Ordinance, hereby grants permission to the City of Alexandria, Virginia, to post placard notice on the property which is the subject of this application.

Kenneth W. Wire

Print Name of Applicant or Agent

Signature

Mailing/Street Address

Telephone #

Fax #

City and State

Zip Code

12/10/2025

Date

### DO NOT WRITE IN THIS SPACE - OFFICE USE ONLY

Application Received: \_\_\_\_\_

Fee Paid: \$ \_\_\_\_\_

Legal advertisement: \_\_\_\_\_

ACTION - PLANNING COMMISSION \_\_\_\_\_

ACTION - CITY COUNCIL: \_\_\_\_\_

MPA # \_\_\_\_\_

REZ # \_\_\_\_\_

**SUBJECT PROPERTY**

Provide the following information for each property for which an amendment is being requested. (Attach separate sheets if needed.)

Address Tax Map - Block - Lot	Land Use Existing - Proposed		Master Plan Designation Existing - Proposed		Zoning Designation Existing - Proposed		Frontage (ft.)	Land Area (acres)
	Office	Residential	Comm	Residential	CDX	CRMU-X		
1 054.04-08-04								6,919 sf
2								
3								
4								

**PROPERTY OWNERSHIP**

☐ Individual Owner ☒ Corporation or Partnership Owner

Identify each person or individual with ownership interest. If corporation or partnership owner, identify each person with more than 3% interest in such corporation or partnership.

- Name: Not-for-profit-corporation Extent of Interest: 100  
 Address: \_\_\_\_\_
- Name: \_\_\_\_\_ Extent of Interest: \_\_\_\_\_  
 Address: \_\_\_\_\_
- Name: \_\_\_\_\_ Extent of Interest: \_\_\_\_\_  
 Address: \_\_\_\_\_
- Name: \_\_\_\_\_ Extent of Interest: \_\_\_\_\_  
 Address: \_\_\_\_\_



**MPA #** \_\_\_\_\_  
**REZ #** \_\_\_\_\_

## **JUSTIFICATION FOR AMENDMENT**

(attach separate sheets if needed)

- 1.** Explain how and why any proposed amendment(s) to the Master Plan are desirable, beneficial to surrounding properties, in character with the applicable Small Area Plan and consistent with City policies:

The proposal is desirable and beneficial, consistent with City policies and in character with the Old Town North Small Area Plan because it creates homes for new residents, reduces obsolete office space, supports cultural and retail activity and brings new investment into the neighborhood in an environmentally sustainable way that also substantially increases the City's tax base. Master Plan height increases matches the
- 2.** Explain how and why the proposed amendment to the Zoning Map(s) is consistent with the proposed amendment to the Master Plan, or, if no amendment to the Master Plan is being requested, how the proposed zoning map amendment is consistent with the existing Master Plan:

Changing the zoning from the commercial CDX to mixed-use CRMU-X advanced the Old Town North Small Area Plan's goals by increasing new housing, reducing outdated office inventory, and increasing the mix of uses in the neighborhood.
- 3.** Explain how the property proposed for reclassification will be served adequately by essential public facilities and services such as highways, streets, parking spaces, police and fire, drainage structures, refuse disposal, water and sewers, and schools.

The project does not expand the building and would reuse existing utilities and public infrastructure, subject to city review. It also frees up existing parking spaces in the building making them available for public parking.
- 4.** If this application is for conditional zoning approval pursuant to Section 11-804 of the Zoning Ordinance, identify all proffered conditions that are to be considered part of this application (see Zoning Ordinance Section 11-804 for restrictions on conditional zoning):

See attached SUP application for residential use.



## APPLICATION

# SPECIAL USE PERMIT

**SPECIAL USE PERMIT #** \_\_\_\_\_

**PROPERTY LOCATION:** 732 North Washington Street, Alexandria, VA 22314

**TAX MAP REFERENCE:** 054.04-08-04 **ZONE:** CDX

**APPLICANT:**

Name: 732-806 Development LLC

Address: \_\_\_\_\_

**PROPOSED USE:** Residential Use with Parking Reduction SUP

- ☒ THE UNDERSIGNED, hereby applies for a Special Use Permit in accordance with the provisions of Article XI, Section 4-11-500 of the 1992 Zoning Ordinance of the City of Alexandria, Virginia.
- ☒ THE UNDERSIGNED, having obtained permission from the property owner, hereby grants permission to the City of Alexandria staff and Commission Members to visit, inspect, and photograph the building premises, land etc., connected with the application.
- ☒ THE UNDERSIGNED, having obtained permission from the property owner, hereby grants permission to the City of Alexandria to post placard notice on the property for which this application is requested, pursuant to Article IV, Section 4-1404(D)(7) of the 1992 Zoning Ordinance of the City of Alexandria, Virginia.
- ☒ THE UNDERSIGNED, hereby attests that all of the information herein provided and specifically including all surveys, drawings, etc., required to be furnished by the applicant are true, correct and accurate to the best of their knowledge and belief. The applicant is hereby notified that any written materials, drawings or illustrations submitted in support of this application and any specific oral representations made to the Director of Planning and Zoning on this application will be binding on the applicant unless those materials or representations are clearly stated to be non-binding or illustrative of general plans and intentions, subject to substantial revision, pursuant to Article XI, Section 11-207(A)(10), of the 1992 Zoning Ordinance of the City of Alexandria, Virginia.

Kenneth Wire

Print Name of Applicant or Agent

\_\_\_\_\_

Mailing/Street Address

\_\_\_\_\_

City and State

Zip Code

Kenneth Wire

Signature

\_\_\_\_\_

Telephone #

\_\_\_\_\_

Email address

12/10/2025

Date

Fax #

### PROPERTY OWNER'S AUTHORIZATION

As the property owner of 732 North Washington Street, I hereby  
(Property Address)  
grant the applicant authorization to apply for the Residential SUP use as  
(use)  
described in this application.

Name: \_\_\_\_\_ Phone \_\_\_\_\_

Please Print

Address: \_\_\_\_\_ Email: \_\_\_\_\_

Signature: \_\_\_\_\_ Date: \_\_\_\_\_

- 1.** Floor Plan and Plot Plan. As a part of this application, the applicant is required to submit a floor plan and plot or site plan with the parking layout of the proposed use. The SUP application checklist lists the requirements of the floor and site plans. The Planning Director may waive requirements for plan submission upon receipt of a written request which adequately justifies a waiver.

☒ **Required floor plan and plot/site plan attached.**

☐ **Requesting a waiver. See attached written request.**

- 2.** The applicant is the (check one):

☐ Owner

☒ Contract Purchaser

☐ Lessee or

☐ Other: \_\_\_\_\_ of the subject property.

State the name, address and percent of ownership of any person or entity owning an interest in the applicant or owner, unless the entity is a corporation or partnership, in which case identify each owner of more than three percent.

No individual owns more than 3% of property owner

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# OWNERSHIP AND DISCLOSURE STATEMENT

Use additional sheets if necessary

1. Applicant. State the name, address and percent of ownership of any person or entity owning an interest in the applicant, unless the entity is a corporation or partnership, in which case identify each owner of more than three percent. The term ownership interest shall include any legal or equitable interest held at the time of the application in the real property which is the subject of the application.

Name	Address	Percent of Ownership
<sup>1.</sup> Patrick Bloomfield		>3%
<sup>2.</sup> Jerry Ricciardi		>3%
<sup>3.</sup> Juan Lopez		>3%

2. Property. State the name, address and percent of ownership of any person or entity owning an interest in the property located at 846 Maryland Drive, Ship P. Richardson, VA 22094 (address), unless the entity is a corporation or partnership, in which case identify each owner of more than three percent. The term ownership interest shall include any legal or equitable interest held at the time of the application in the real property which is the subject of the application.

Name	Address	Percent of Ownership
<sup>1.</sup> None		N/A
<sup>2.</sup>		
<sup>3.</sup>		

3. Business or Financial Relationships. Each person or entity indicated above in sections 1 and 2, with an ownership interest in the applicant or in the subject property are required to disclose **any** business or financial relationship, as defined by [Section 11-350 of the Zoning Ordinance](#), existing at the time of this application, or within the 12-month period prior to the submission of this application with any member of the Alexandria City Council, Planning Commission, Board of Zoning Appeals or either Boards of Architectural Review. **All fields must be filled out completely. Do not leave blank. (If there are no relationships please indicate each person or entity and "None" in the corresponding fields).**

For a list of current council, commission and board members, as well as the definition of business and financial relationship, [click here](#).

Name of person or entity	Relationship as defined by Section 11-350 of the Zoning Ordinance	Member of the Approving Body (i.e. City Council, Planning Commission, etc.)
<sup>1.</sup> none	none	none
<sup>2.</sup>		
<sup>3.</sup>		

**NOTE: Business or financial relationships of the type described in Sec. 11-350 that arise after the filing of this application and before each public hearing must be disclosed prior to the public hearings.**

As the applicant or the applicant's authorized agent, I hereby attest to the best of my ability that the information provided above is true and correct.

12/10/2025

Kenneth Wire

Date

Printed Name

Signature

Form Revised November 2019

Last updated: 10.21.2020



## USE CHARACTERISTICS

4. The proposed special use permit request is for (*check one*):

- ☒ a new use requiring a special use permit,  
☐ an expansion or change to an existing use without a special use permit,  
☐ an expansion or change to an existing use with a special use permit,  
☐ other. Please describe: \_\_\_\_\_

5. Please describe the capacity of the proposed use:

A. How many patrons, clients, pupils and other such users do you expect?  
Specify time period (i.e., day, hour, or shift).

~~Typical for residential~~ \_\_\_\_\_  
\_\_\_\_\_

B. How many employees, staff and other personnel do you expect?  
Specify time period (i.e., day, hour, or shift).

~~As needed for maintenance~~ \_\_\_\_\_  
\_\_\_\_\_

6. Please describe the proposed hours and days of operation of the proposed use:

Day:  
Seven days per week

Hours:  
up to 24 hours

\_\_\_\_\_  
\_\_\_\_\_  
\_\_\_\_\_  
\_\_\_\_\_

\_\_\_\_\_  
\_\_\_\_\_  
\_\_\_\_\_  
\_\_\_\_\_

7. Please describe any potential noise emanating from the proposed use.

A. Describe the noise levels anticipated from all mechanical equipment and patrons.

~~Typical for residential~~ \_\_\_\_\_  
\_\_\_\_\_

B. How will the noise be controlled?

~~Property management~~ \_\_\_\_\_  
\_\_\_\_\_

- 8.** Describe any potential odors emanating from the proposed use and plans to control them:

Property Management

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- 9.** Please provide information regarding trash and litter generated by the use.

- A. What type of trash and garbage will be generated by the use? (i.e. office paper, food wrappers)

Typical for residential

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- B. How much trash and garbage will be generated by the use? (i.e. # of bags or pounds per day or per week)

Typical for residential

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- C. How often will trash be collected?

As needed

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- D. How will you prevent littering on the property, streets and nearby properties?

Property management

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- 10.** Will any hazardous materials, as defined by the state or federal government, be handled, stored, or generated on the property?

[ ] Yes.

[✓] No.

If yes, provide the name, monthly quantity, and specific disposal method below:

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- 1.** Will any organic compounds, for example paint, ink, lacquer thinner, or cleaning or degreasing solvent, be handled, stored, or generated on the property?

☐ Yes.      ☒ No.

If yes, provide the name, monthly quantity, and specific disposal method below:

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- 12.** What methods are proposed to ensure the safety of nearby residents, employees and patrons?  
Property management

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## **ALCOHOL SALES**

- 13.** A. Will the proposed use include the sale of beer, wine, or mixed drinks?

☐ Yes      ☒ No

If yes, describe existing (if applicable) and proposed alcohol sales below, including if the ABC license will include on-premises and/or off-premises sales.

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## PARKING AND ACCESS REQUIREMENTS

14. A. How many parking spaces of each type are provided for the proposed use:

See plans Standard spaces  
\_\_\_\_\_ Compact spaces  
\_\_\_\_\_ Handicapped accessible spaces.  
\_\_\_\_\_ Other.

Planning and Zoning Staff Only
Required number of spaces for use per Zoning Ordinance Section 8-200A _____
Does the application meet the requirement? [ ] Yes [ ] No

- B. Where is required parking located? (*check one*)

[✓] on-site

[ ] off-site

If the required parking will be located off-site, where will it be located?

---

**PLEASE NOTE:** Pursuant to Section 8-200 (C) of the Zoning Ordinance, commercial and industrial uses may provide off-site parking within 500 feet of the proposed use, provided that the off-site parking is located on land zoned for commercial or industrial uses. All other uses must provide parking on-site, except that off-street parking may be provided within 300 feet of the use with a special use permit.

- C. If a reduction in the required parking is requested, pursuant to Section 8-100 (A) (4) or (5) of the Zoning Ordinance, complete the PARKING REDUCTION SUPPLEMENTAL APPLICATION.

[ ] **Parking reduction requested; see attached supplemental form**

15. Please provide information regarding loading and unloading facilities for the use:

- A. How many loading spaces are available for the use? 0

Planning and Zoning Staff Only
Required number of loading spaces for use per Zoning Ordinance Section 8-200 _____
Does the application meet the requirement? [ ] Yes [ ] No

- B. Where are off-street loading facilities located? \_\_\_\_\_  
 \_\_\_\_\_
- C. During what hours of the day do you expect loading/unloading operations to occur?  
As allowed per Noise Ordinance  
 \_\_\_\_\_
- D. How frequently are loading/unloading operations expected to occur, per day or per week, as appropriate?  
Typical for residential  
 \_\_\_\_\_

- 16.** Is street access to the subject property adequate or are any street improvements, such as a new turning lane, necessary to minimize impacts on traffic flow?

Existing street access is adequate.  
 \_\_\_\_\_  
 \_\_\_\_\_

## **SITE CHARACTERISTICS**

- 17.** Will the proposed uses be located in an existing building? ☒ Yes ☐ No
- Do you propose to construct an addition to the building? ☐ Yes ☒ No
- How large will the addition be? \_\_\_\_\_ square feet.

- 18.** What will the total area occupied by the proposed use be?

\_\_\_\_\_ sq. ft. (existing) + \_\_\_\_\_ sq. ft. (addition if any) = \_\_\_\_\_ sq. ft. (total)

- 19.** The proposed use is located in: *(check one)*

- ☒ a stand alone building  
☐ a house located in a residential zone  
☐ a warehouse  
☐ a shopping center. Please provide name of the center: \_\_\_\_\_  
☐ an office building. Please provide name of the building: \_\_\_\_\_  
☐ other. Please describe: \_\_\_\_\_

**End of Application**



# APPLICATION - SUPPLEMENTAL

## PARKING REDUCTION

**Supplemental information to be completed by applicants requesting special use permit approval of a reduction in the required parking pursuant to section 8-100(A)(4) or (5).**

**1. Describe the requested parking reduction.** (e.g. number of spaces, stacked parking, size, off-site location)

The Applicant requests a parking reduction in order to provide 15 parking spaces where 20 are required under the zoning ordinance. The plan, however, does include 9 additional tandem parking spaces for a total of 24 actual parking spaces provided.

**2. Provide a statement of justification for the proposed parking reduction.**

When including the tandem parking spaces, the total provided parking spaces of 24 spaces exceeds the zoning requirements and will be adequate to meet demand.

**3. Why is it not feasible to provide the required parking?**

The Applicant is proposing to convert an existing office building and cannot add new parking spaces to the project.

**4. Will the proposed reduction reduce the number of available parking spaces below the number of existing parking spaces?**

\_\_\_\_\_ Yes.      ☒ No.

5. If the requested reduction is for more than five parking spaces, the applicant must submit a **Parking Management Plan** which identifies the location and number of parking spaces both on-site and off-site, the availability of on-street parking, any proposed methods of mitigating negative affects of the parking reduction.

6. The applicant must also demonstrate that the reduction in parking will not have a negative impact on the surrounding neighborhood.



## APPLICATION

### ENCROACHMENT

**PROPERTY LOCATION:** 732 North Washington Street, Alexandria Virginia 22314

**TAX MAP REFERENCE:** 054.04-08-04

**ZONE:** CDX

#### APPLICANT

Name: 732-806 Development LLC

Address:

#### PROPERTY OWNER

Name: American Statistical Association

Address:

**PROPOSED USE:** Encroachment for above grade balconies and at grade planters

**INSURANCE CARRIER** (copy attached) \_\_\_\_\_ **POLICY #** \_\_\_\_\_

A certificate of general liability insurance in the amount of \$1,000,000 which will indemnify the owner and names the city as an additional insured must be attached to this application.

- ☒ THE UNDERSIGNED hereby applies for an Encroachment Ordinance in accordance with the provisions of Section 8-1-16 and Sections 3-2-82 and 85 of the Code of the City of Alexandria, Virginia.
- ☒ THE UNDERSIGNED, having obtained permission from the property owner, hereby grants permission to the City of Alexandria staff and Commission Members to visit, inspect, and photograph the building premises, land etc., connected with the application.
- ☒ THE UNDERSIGNED, having obtained permission from the property owner, hereby grants permission to the City of Alexandria to post placard notice on the property for which this application is requested, pursuant to Article XI, Section 11-301 (B) of the 1992 Zoning Ordinance of the City of Alexandria, Virginia.
- ☒ THE UNDERSIGNED also attests that all of the information herein provided and specifically including all surveys, drawings, etc., required of the applicant are true, correct and accurate to the best of their knowledge and belief.

Kenneth W. Wire, Wire Gill LLP

Print Name of Applicant or Agent

Mailing/Street Address

City and State

Zip Code

Signature

Telephone #

Fax #

Email address

October 3, 2025

Date

# OWNERSHIP AND DISCLOSURE STATEMENT

Use additional sheets if necessary

1. Applicant. State the name, address and percent of ownership of any person or entity owning an interest in the applicant, unless the entity is a corporation or partnership, in which case identify each owner of more than three percent. The term ownership interest shall include any legal or equitable interest held at the time of the application in the real property which is the subject of the application.

Name	Address	Percent of Ownership
1. Patrick Bloomfield		>3%
2. Jerry Ricciardi		>3%
3. Juan Lopez		>3%

2. Property. State the name, address and percent of ownership of any person or entity owning an interest in the property located at 732 N. Washington Street (address), unless the entity is a corporation or partnership, in which case identify each owner of more than three percent. The term ownership interest shall include any legal or equitable interest held at the time of the application in the real property which is the subject of the application.

Name	Address	Percent of Ownership
1. None		
2.		
3.		

3. Business or Financial Relationships. Each person or entity indicated above in sections 1 and 2, with an ownership interest in the applicant or in the subject property are required to disclose **any** business or financial relationship, as defined by [Section 11-350 of the Zoning Ordinance](#), existing at the time of this application, or within the 12-month period prior to the submission of this application with any member of the Alexandria City Council, Planning Commission, Board of Zoning Appeals or either Boards of Architectural Review. **All fields must be filled out completely. Do not leave blank. (If there are no relationships please indicate each person or entity and "None" in the corresponding fields).**

For a list of current council, commission and board members, as well as the definition of business and financial relationship, [click here](#).

Name of person or entity	Relationship as defined by Section 11-350 of the Zoning Ordinance	Member of the Approving Body (i.e. City Council, Planning Commission, etc.)
1. None	None	N/A
2.		
3.		

**NOTE: Business or financial relationships of the type described in Sec. 11-350 that arise after the filing of this application and before each public hearing must be disclosed prior to the public hearings.**

As the applicant or the applicant's authorized agent, I hereby attest to the best of my ability that the information provided above is true and correct.

October 3, 2025

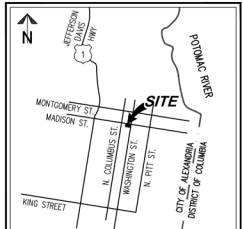
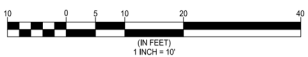
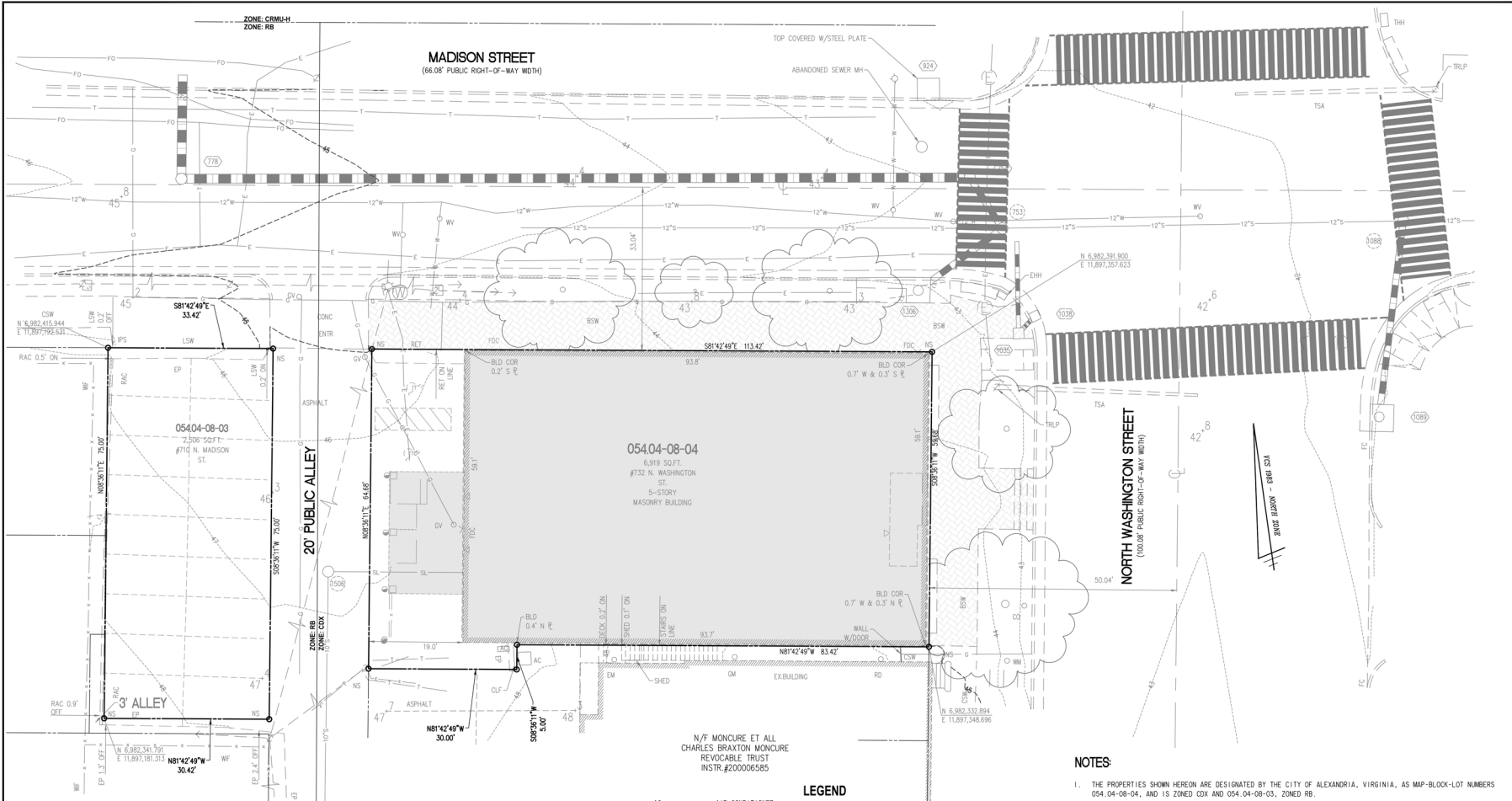
Date

Kenneth W. Wire, Wire Gill LLP

Printed Name



Signature



VICINITY MAP SCALE: 1"=2000'

LEGEND

- |  |                                    |
|--|------------------------------------|
| AC.....AIR CONDITIONER                   | SD.....STORM SEWER STRUCTURE       |
| APPROX. LOC.....APPROXIMATE LOCATION     | SDH.....SANITARY SEWER STRUCTURE   |
| BRL.....BUILDING RESTRICTION LINE        | SO FT.....SQUARE FEET              |
| BSW.....BRICK SIDEWALK                   | TH.....TELECOMMUNICATIONS HANDHOLE |
| CLF.....CHAIN LINK FENCE                 | TRLP.....TRAFFIC LIGHT/SIGNAL POLE |
| CO.....CLEANOUT                          | TSA.....TRAFFIC SIGNAL ARM         |
| CONC.....CONCRETE                        | WDF.....WOODEN FENCE               |
| CONSTR.....CONSTRUCTION                  | WIF.....IRON FENCE                 |
| CSW.....CONCRETE SIDEWALK                | WM.....WATER METER                 |
| DB.....DEED BOOK                         | WV.....WATER VALVE                 |
| DE.....DAMPSTER ENCLOSURE                | WV.....WATER VALVE                 |
| EG.....EDGE OF GRAVEL                    | WV.....WATER VALVE                 |
| EDH.....ELECTRIC HANDHOLE                | WV.....WATER VALVE                 |
| EM.....ELECTRIC METER                    | WV.....WATER VALVE                 |
| EP.....EDGE OF PAVEMENT                  | WV.....WATER VALVE                 |
| EPVR.....EDGE OF PAVEMENT                | WV.....WATER VALVE                 |
| ESMT.....EASEMENT                        | WV.....WATER VALVE                 |
| FC.....HEADER CURB                       | WV.....WATER VALVE                 |
| FDC.....FIRE DEPARTMENT CONNECTION       | WV.....WATER VALVE                 |
| FF.....FIRST/FINISH FLOOR ELEVATION      | WV.....WATER VALVE                 |
| GM.....GAS METER                         | WV.....WATER VALVE                 |
| GV.....GAS VALVE                         | WV.....WATER VALVE                 |
| INSTR.#.....INSTRUMENT NUMBER            | WV.....WATER VALVE                 |
| INV.....INVERT                           | WV.....WATER VALVE                 |
| IPF.....IRON PIN FOUND (PROPERTY CORNER) | WV.....WATER VALVE                 |
| IPS.....IRON PIN SET (PROPERTY CORNER)   | WV.....WATER VALVE                 |
| N/F.....NOW OR FORMERLY                  | WV.....WATER VALVE                 |
| NS.....NAIL SET (PROPERTY CORNER)        | WV.....WATER VALVE                 |
| OH.....OVERHANG/CANOPY                   | WV.....WATER VALVE                 |
| PAGE.....PAGE                            | WV.....WATER VALVE                 |
| RAC.....ROLLED ASPHALT CURB              | WV.....WATER VALVE                 |
| RCP.....REINFORCED CONCRETE PIPE         | WV.....WATER VALVE                 |
| RE.....RECEIVED ENTRANCE                 | WV.....WATER VALVE                 |
| RET.....RETAINING WALL                   | WV.....WATER VALVE                 |

NOTES:

- THE PROPERTIES SHOWN HEREON ARE DESIGNATED BY THE CITY OF ALEXANDRIA, VIRGINIA, AS MAP-BLOCK-LOT NUMBERS 054.04-08-04, AND IS ZONED CDX AND 054.04-08-03, ZONED RB.
- THE PROPERTIES ARE NOW IN THE NAME OF AMERICAN STATISTICAL ASSOCIATION, AS RECORDED INSTRUMENT NUMBER 050010199 AMONG THE LAND RECORDS OF THE CITY OF ALEXANDRIA, VIRGINIA.
- THIS PLAT AND THE SURVEY UPON WHICH IT IS BASED SHOWS ONLY THOSE IMPROVEMENTS THAT ARE OBSERVABLE AND CAN BE LOCATED USING NORMAL SURVEY METHODS. THE UNDERGROUND UTILITIES SHOWN HAVE BEEN LOCATED FROM FIELD SURVEY INFORMATION, MISS UTILITY MARKINGS AND EXISTING RECORDS; THERE ARE NO GUARANTEES, EITHER EXPRESS OR IMPLIED, THAT THE UNDERGROUND UTILITIES SHOWN COMPRISE ALL SUCH UTILITIES IN THE AREA, EITHER IN SERVICE OR ABANDONED, OR THAT THE UNDERGROUND UTILITIES SHOWN ARE IN THE EXACT LOCATION INDICATED. THE UNDERGROUND UTILITIES HAVE NOT BEEN PHYSICALLY LOCATED. WATER AND GAS LINE SIZES ARE FROM RECORD INFORMATION.
- THE DEPICTION OF OVERHEAD WIRES SHOWN HEREON REPRESENT THE PRESENCE OF OVERHEAD UTILITIES BUT DOES NOT REFLECT ACTUAL WIRE LOCATIONS OR SERVICES THAT MAY EXIST BETWEEN INDIVIDUAL POLES OR STRUCTURES.
- TOTAL AREA OF THE PROPERTIES IS 9,425 SQUARE FEET OR 0.2164 ACRES.
- THIS PLAT IS BASED ON A FIELD SURVEY BY THIS FIRM, DATED 07/25/2024.
- THE FEDERAL EMERGENCY MANAGEMENT AGENCY'S FLOOD INSURANCE RATE MAP FOR THE CITY OF ALEXANDRIA, VIRGINIA, MAP NUMBER 515519003ZF, MAP REVISED DATE JANUARY 11, 2024, DESIGNATES THE PROPERTY AS BEING IN ZONE X, AREAS OF MINIMAL FLOOD HAZARD.
- EASEMENTS, CONDITIONS, COVENANTS AND RESTRICTIONS, SHOWN AND/OR NOTED, ARE PER THE TITLE REPORT PREPARED BY PHILIP E. TIAN/LOS T/A EXAMINERS TITLE & ABSTRACT, LLC, COUNSELORS NUMBER CTO134910-SER, EFFECTIVE DATE MAY 24, 2025.
- THE SITE SHOWN HEREON IS REFERENCED TO THE NORTH AMERICAN VERTICAL DATUM OF 1988 AS COMPUTED FROM A FIELD RUN VERTICAL CONTROL SURVEY AND IS REFERENCED TO THE VIRGINIA COORDINATE SYSTEM OF 1983. THE COMBINED FACTOR APPLIED TO THE FIELD DISTANCES TO DERIVE THE REFERENCED COORDINATES IS 0.99995695. THE FOOT DEFINITION USED IN THE PERFORMANCE OF THIS SURVEY IS THE U.S. SURVEY FOOT. CONTOUR INTERVAL IS ONE FOOT.
- THIS SURVEY WAS COMPLETED UNDER THE DIRECT AND RESPONSIBLE CHARGE OF, DAVID N. ISHERWOOD, L.S., FROM AN ACTUAL [X] GROUND OR [ ] AIRBORNE SURVEY MADE UNDER MY SUPERVISION; THAT THE IMAGERY AND/OR ORIGINAL DATA WAS OBTAINED ON JULY 25, 2024; AND THAT THIS PLAT, MAP, OR DIGITAL GEOSPATIAL DATA INCLUDING METADATA MEETS MINIMUM ACCURACY STANDARDS UNLESS OTHERWISE NOTED.

DEVELOPMENT TEAM INFORMATION

- OWNER**  
AMERICAN STATISTICAL ASSOCIATION  
732 N. WASHINGTON ST  
ALEXANDRIA, VA 22314
- APPLICANT:**  
PT BLOOMS DEVELOPMENT  
509 WYTHIE STREET  
ALEXANDRIA, VA 22314  
ATTN: PATRICK BLOOMFIELD
- CIVIL ENGINEER:**  
WALTER L. PHILLIPS, INC.  
207 PARK AVENUE  
FALLS CHURCH, VA 22046  
ATTN: TRAVIS P. BROWN, P.E.
- ARCHITECT:**  
JONATHAN KUBIN ARCHITECT  
608 KENNEDY STREET NW, SUITE 313  
WASHINGTON, DC 20001  
ATTN: JONATHAN KUBIN
- LAND USE ATTORNEY:**  
WIRE GILL, LLP  
1750 TYSONS BLVD, SUITE 1500  
TYSONS, VA 22101  
ATTN: KEIN WIRE

**WALTER L. PHILLIPS, INC.**  
INCORPORATED  
ESTABLISHED 1940  
Engineers • Surveyors • Planners • Landscape Architects • Architects  
207 PARK AVENUE FALLS CHURCH, VIRGINIA 22046  
(703) 524-6163 Fax (703) 533-1501 www.WLPHINC.com

SCALE: 1" = 10'  
DATE: 08/05/25  
SPECIAL USE PERMIT 08/05/25  
SPECIAL USE PERMIT 08/05/25



REVISION APPROVED BY		DATE	
NO.	DESCRIPTION	DATE	APPROVED

**AMERICAN STATISTICAL ASSOCIATION ON WASHINGTON**  
**732 NORTH WASHINGTON STREET**  
**SPECIAL USE PERMIT**  
**CITY OF ALEXANDRIA, VIRGINIA**  
**EXISTING CONDITIONS PLAN**

**APPROVED SPECIAL USE PERMIT NO.** \_\_\_\_\_  
**DEPARTMENT OF PLANNING & ZONING**

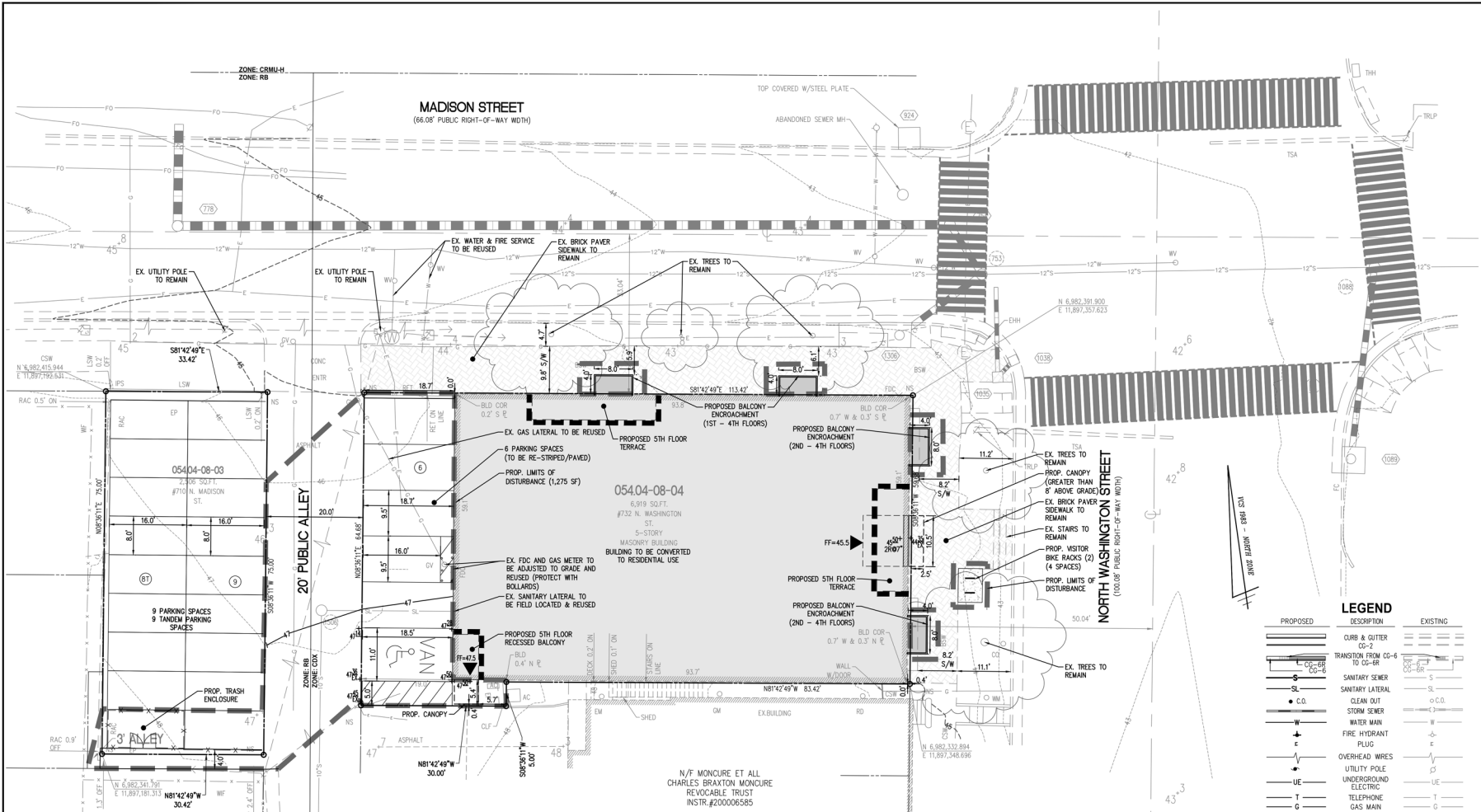
DIRECTOR	DATE
SITE PLAN NO.	

DIRECTOR \_\_\_\_\_ DATE \_\_\_\_\_

CHAIRMAN, PLANNING COMMISSION	DATE
DATE RECORDED	

INSTRUMENT NO. \_\_\_\_\_ DEED BOOK NO. \_\_\_\_\_ PAGE NO. \_\_\_\_\_





### ZONING TABULATIONS

**ZONING:** EXISTING ZONE: CDX/RB, PROPOSED ZONE: CRMU-X  
**MASTER PLAN:** OLD TOWN NORTH SMALL AREA PLAN  
**SITE AREA (SQ. FT. ACRES):** 6,919 SQ. FT. OR 0.1588 ACRES (BLDG); 2,506 SQ. FT. OR 0.0575 ACRES (PARKING)  
**USE:** EXISTING: OFFICE/PARKING LOT  
PROPOSED: RESIDENTIAL MULTIFAMILY

REQUIREMENT	REQUIRED	PROVIDED
DENSITY (UNITS/ACRE)	CRMU-X	125.94 (20 DWELLING UNITS/0.1588 ACRES)
FAR	2.5 w/ SUP	2.4556
GROSS FLOOR AREA (SF)	N/A	26522
FLOOR AREA (SF)	25662.5	23144
LOT AREA (SF)	N/A (INCLUDES BOTH LOTS)	9425
SETBACKS (FT)		
FRONT - N. WASHINGTON ST.	N/A	0.4'
FRONT - MADISON	N/A	0.0'
SIDE	N/A	0.0', 18.7'
REAR	N/A	N/A
FRONTAGE/LOT WIDTH (FT)	50 FT	59.68 Washington St., 114.42' Madison St.
OPEN SPACE (SF)	25% TM054.04-08-04 (1730 SF)	1730 (25%)
GROUND LEVEL	---	---
ROOFTOP	---	1730 (25%)
AVERAGE FINISHED GRADE	---	EX. GRADES
HEIGHT (FT)	50 FT (MAX)	53.6 FT
PARKING	1 SP/BECDROOM (2 MAX/UNIT)	15 (PLUS 8 TANDEN)
TRIP GENERATION	---	SEE CALC THIS SHEET
TREE CANOPY COVERAGE	25%	0%

### DEVELOPMENT TABULATIONS

UNIT TYPE	UNITS
1-BEDROOM	11
2-BEDROOM	6
3-BEDROOM	1
TOTAL	18

\*THE EXISTING BUILDING IS TO BE RETAINED AND CONVERTED TO RESIDENTIAL USE(S). THE BUILDING HEIGHT IS TO BE INCREASED UNDER A SEPARATE MASTER PLAN AMENDMENT.

### PARKING TABULATION

UNIT TYPE	UNITS	BEDROOMS	PARKING RATE (SP/UNIT)	ADMIN REDUCTION (LESS 15%)*	PARKING REQUIRED (SPACES)
1-BEDROOM	11	1	0.8	0.85	7.48
2-BEDROOM	6	2	0.8	0.85	10.08
3-BEDROOM	1	2	0.8	0.85	1.36
TOTAL					19.72
MAXIMUM PARKING ALLOWED = 20 UNITS X 2 SP/UNIT = 40 SPACES					
PARKING PROVIDED					
STANDARD/ADA	5				
COMPACT	30				
TANDEN	8				
TOTAL	43				
TOTAL 15 SPACES (PLUS 8 TANDEN SPACES NOT COUNTED FOR ZONING COMPLIANCE)					

### TRIP GENERATION ANALYSIS

PROVIDED USING "INSTITUTE OF TRANSPORTATION ENGINEERS: TRIP GENERATION"  
EXISTING TRIPS (OFFICE/PARKING LOT) = 27,421 SF/1000 SF X 11.01 = 302 TRIPS/DAY  
PROPOSED TRIPS (MULTIFAMILY\*\*) = 20 UNITS X 6.72 = 135 TRIPS/DAY  
\*ITE RATE FOR GENERAL OFFICE BUILDING = 11.01 TRIPS/DAY/1000 SF GFA  
\*\*ITE RATE FOR APARTMENT = 6.72 TRIPS/DAY/UNIT

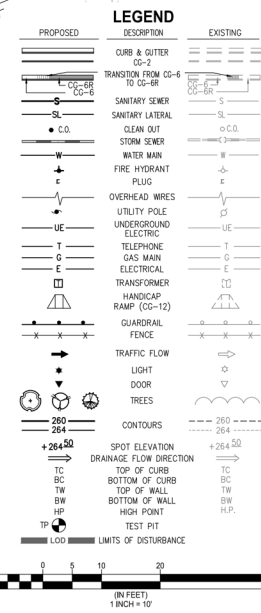
### PROJECT DESCRIPTION NARRATIVE

THE APPLICANT SEEKS TO CONVERT THE EXISTING OFFICE BUILDING TO MULTIFAMILY RESIDENTIAL USE THROUGH A REZONING AND SUP PROCESS FOR CONVERSION OF OFFICE TO MULTIFAMILY RESIDENTIAL. THE APPLICANT SEEKS TO UTILIZE THE ADJACENT LOT, 710 MADISON STREET, FOR SITE AREA, DENSITY PURPOSES AND FOR PARKING. MINIMAL SITE CHANGES ARE REQUESTED.

THE PROPERTY IS LOCATED IN "OLD AND HISTORIC ALEXANDRIA" PER THE CITY OF ALEXANDRIA HISTORIC PRESERVATION MAP.

### SPECIAL USE PERMITS/ZONING MODIFICATIONS/WAIVERS

- 1) REZONING FROM CDX/RB TO CRMU-X
- 2) EX. APPROVED SITE PLAN #63-040; PROPOSED REQUEST FOR A SPECIAL USE PERMIT FOR CONVERSION OF OFFICE TO MULTIFAMILY RESIDENTIAL USE.
- 3) SPECIAL USE PERMIT FOR PARKING REDUCTION FROM MINIMUM REQUIREMENT (20 SPACES) TO 15 SPACES PLUS 8 TANDEN SPACES. (Z.O. 8-200)
- 4) MODIFICATION TO MINIMUM TREE CANOPY COVERAGE. (Z.O. 7-2200)
- 5) RIGHT-OF-WAY ENCROACHMENT ORDINANCE FOR FOUR (4) VARIABLE DEPTH X 8' WIDE BALCONIES.
- 6) MASTER PLAN AMENDMENT REQUESTED TO INCREASE HEIGHT UNDER SEPARATE APPLICATION.



## AMERICAN STATISTICAL ASSOCIATION ON WASHINGTON

### 732 NORTH WASHINGTON STREET

### SPECIAL USE PERMIT

CITY OF ALEXANDRIA, VIRGINIA

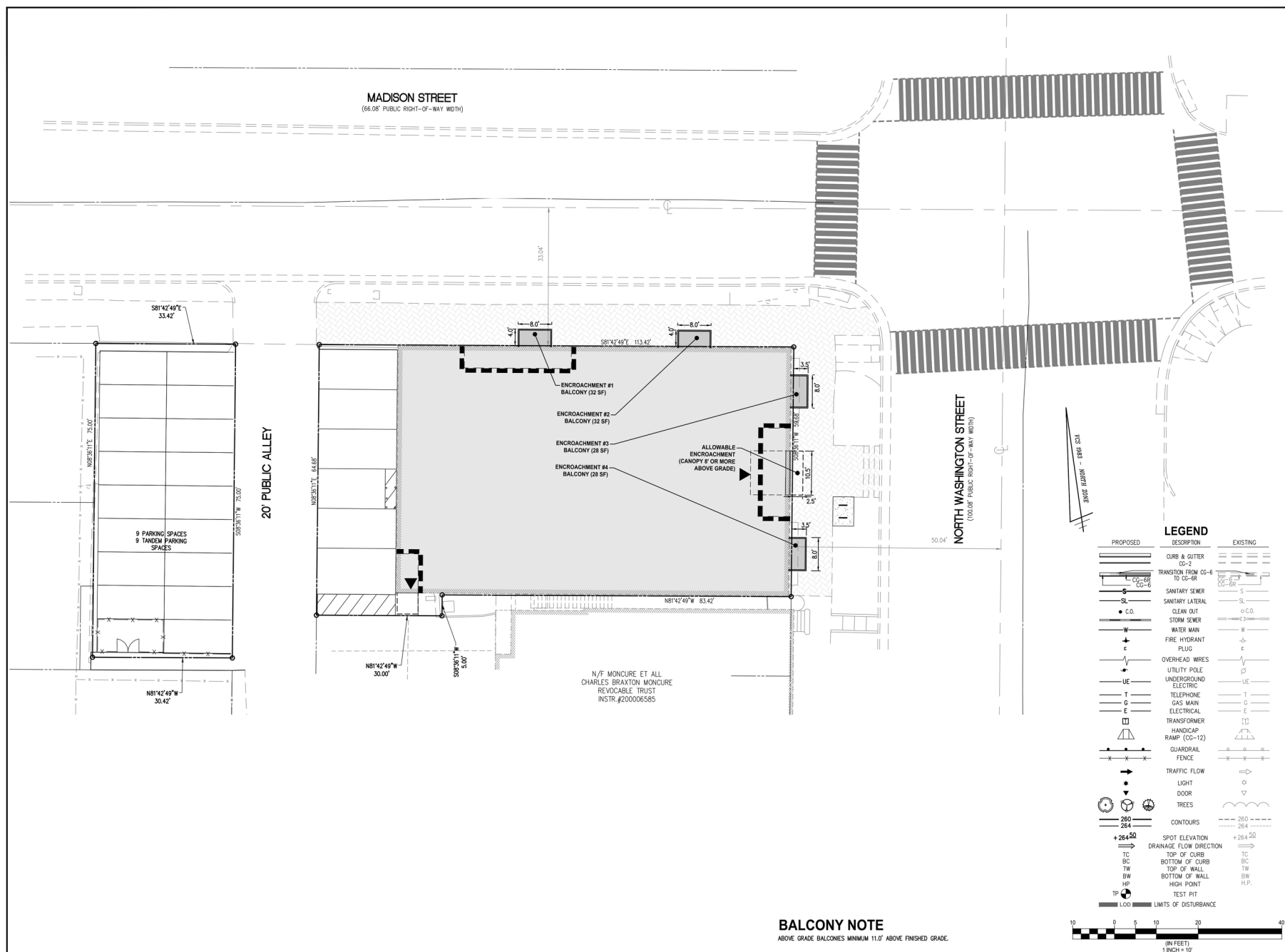
APPROVED SPECIAL USE PERMIT NO. _____	
DEPARTMENT OF PLANNING & ZONING	
DIRECTOR _____	DATE _____
DEPARTMENT OF TRANSPORTATION & ENVIRONMENTAL SERVICES	
SITE PLAN No. _____	
DIRECTOR _____	DATE _____
CHAIRMAN, PLANNING COMMISSION _____	
DATE RECORDED _____	
INSTRUMENT NO. _____	DEED BOOK NO. _____
PAGE NO. _____	

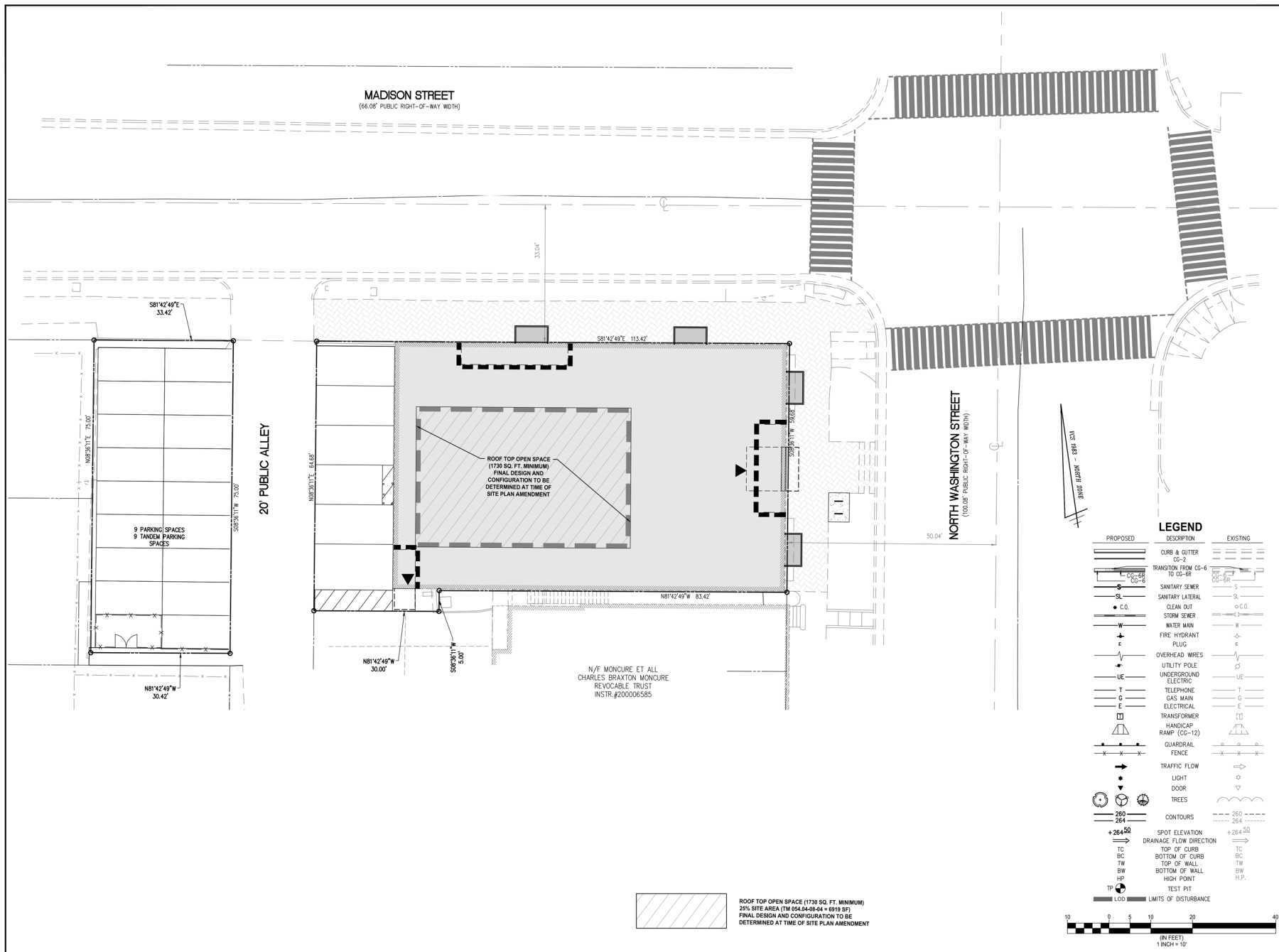
**WALTER L. PHILLIPS INCORPORATED**  
ESTABLISHED 1946  
Engineers • Surveyors • Planners • Landscape Architects • Architects  
207 PARK AVENUE FALLS CHURCH, VIRGINIA 22046  
(703) 524-8183 Fax (703) 524-1501 www.WLPHINC.com

SCALE: 1" = 40'  
DATE: 09/05/23  
DRAWN: [ ]  
CHECKED: [ ]  
SPECIAL USE PERMIT 09/05/23  
SPECIAL USE PERMIT 09/05/23

NO.	DESCRIPTION	DATE	APPROVED

### PROPOSED CONDITIONS PLAN





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(703) 524-8183 Fax (703) 524-1501 www.WLPHINC.com

DATE: 09/05/23  
SCALE: 1" = 10'

**AMERICAN STATISTICAL ASSOCIATION ON WASHINGTON**  
732 NORTH WASHINGTON STREET  
SPECIAL USE PERMIT  
CITY OF ALEXANDRIA, VIRGINIA

**OPEN SPACE EXHIBIT**

REVISION APPROVED BY			
NO.	DESCRIPTION	DATE	APPROVED

**APPROVED**  
SPECIAL USE PERMIT No. \_\_\_\_\_  
DEPARTMENT OF PLANNING & ZONING

DIRECTOR \_\_\_\_\_ DATE \_\_\_\_\_  
DEPARTMENT OF TRANSPORTATION & ENVIRONMENTAL SERVICES  
SITE PLAN No. \_\_\_\_\_  
DIRECTOR \_\_\_\_\_ DATE \_\_\_\_\_

CHAIRMAN, PLANNING COMMISSION \_\_\_\_\_ DATE \_\_\_\_\_  
DATE RECORDED \_\_\_\_\_  
INSTRUMENT NO. \_\_\_\_\_ DEED BOOK NO. \_\_\_\_\_ PAGE NO. \_\_\_\_\_

JK<sub>A</sub>

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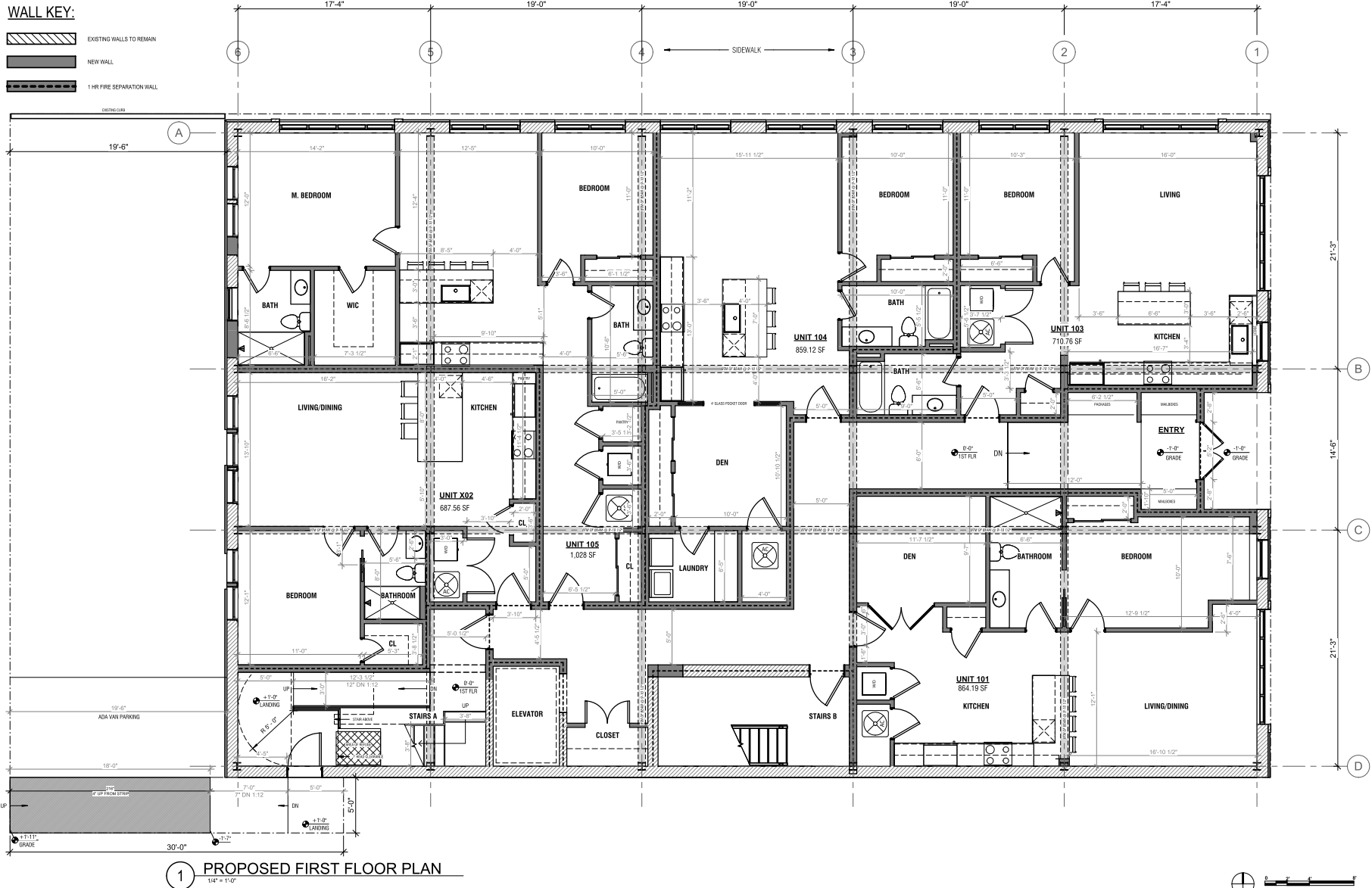
SEAL

SHEET NAME  
PROPOSED FIRST  
FLOOR PLAN

SCALE  
1/4" = 1'-0"

SHEET NO.

A101



**JK<sub>A</sub>**

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SEAL

SHEET NAME  
PROPOSED TYPICAL  
FLOOR PLAN - V5

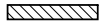

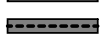
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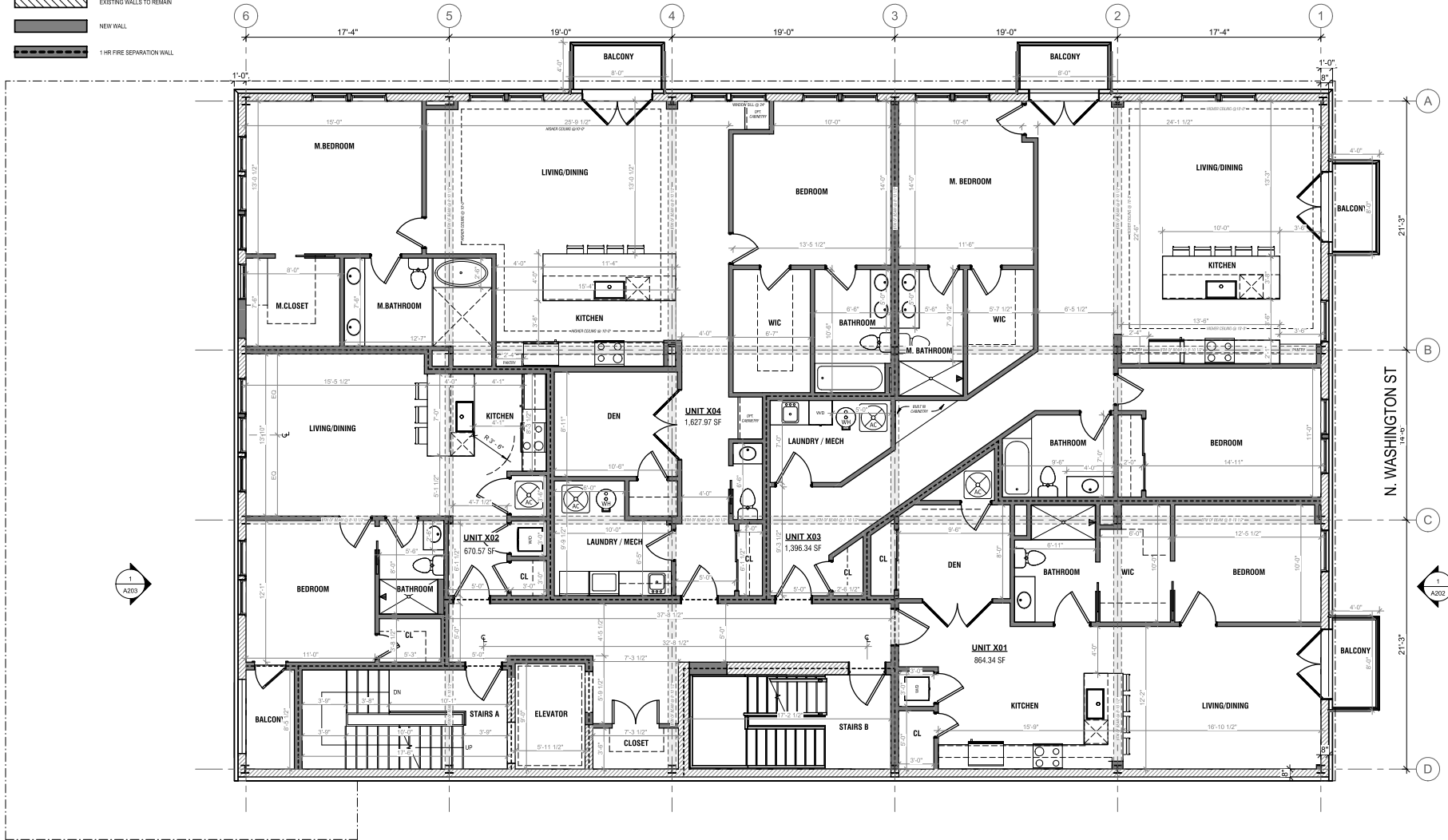
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**A102**

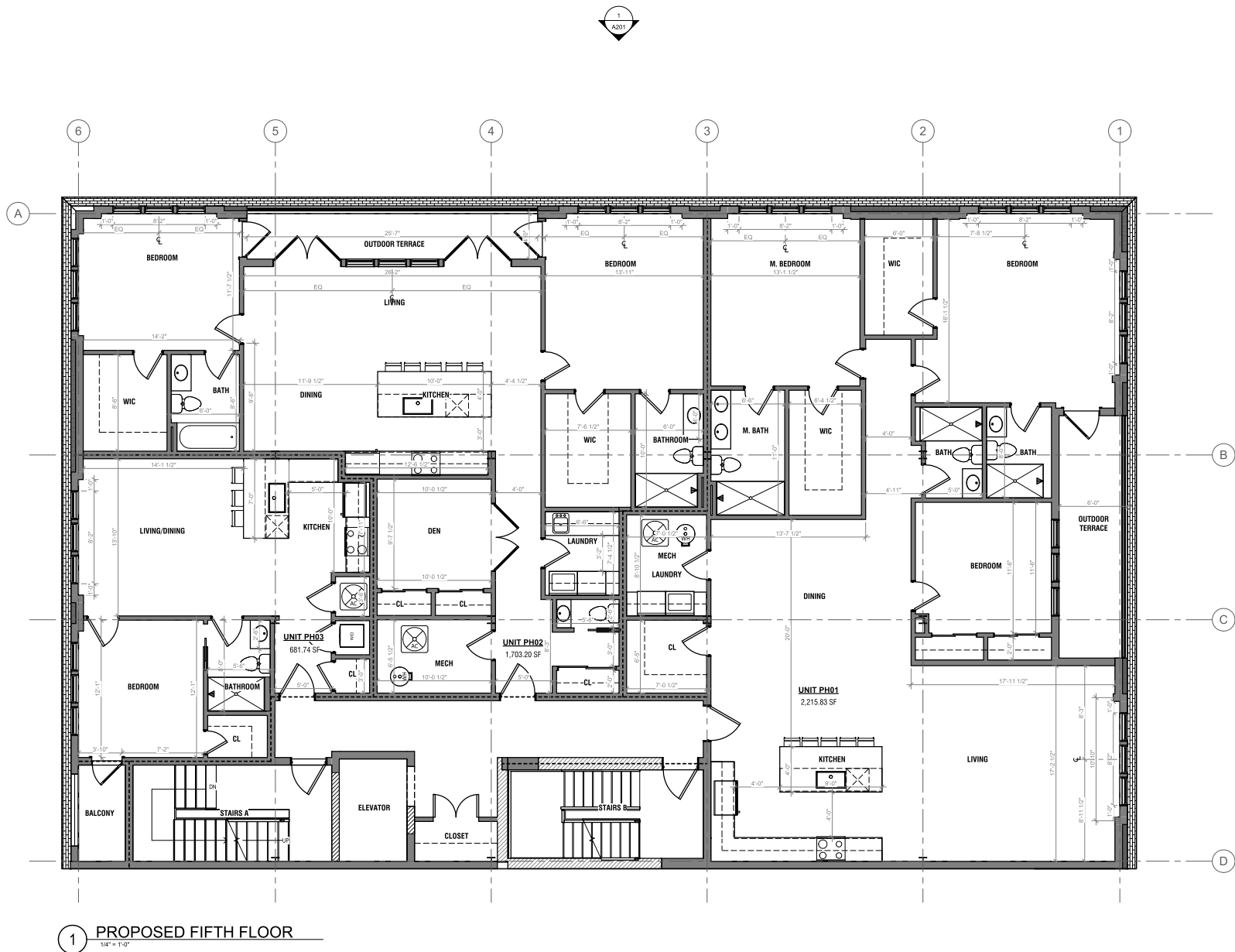
MADISON ST

**WALL KEY:**

-  EXISTING WALLS TO REMAIN
-  NEW WALL
-  1 HR FIRE SEPARATION WALL



**1** PROPOSED SECOND FLOOR PLAN (TYPICAL)  
1/4" = 1'-0"



OFFICIAL STAMPS

**JK<sub>A</sub>**

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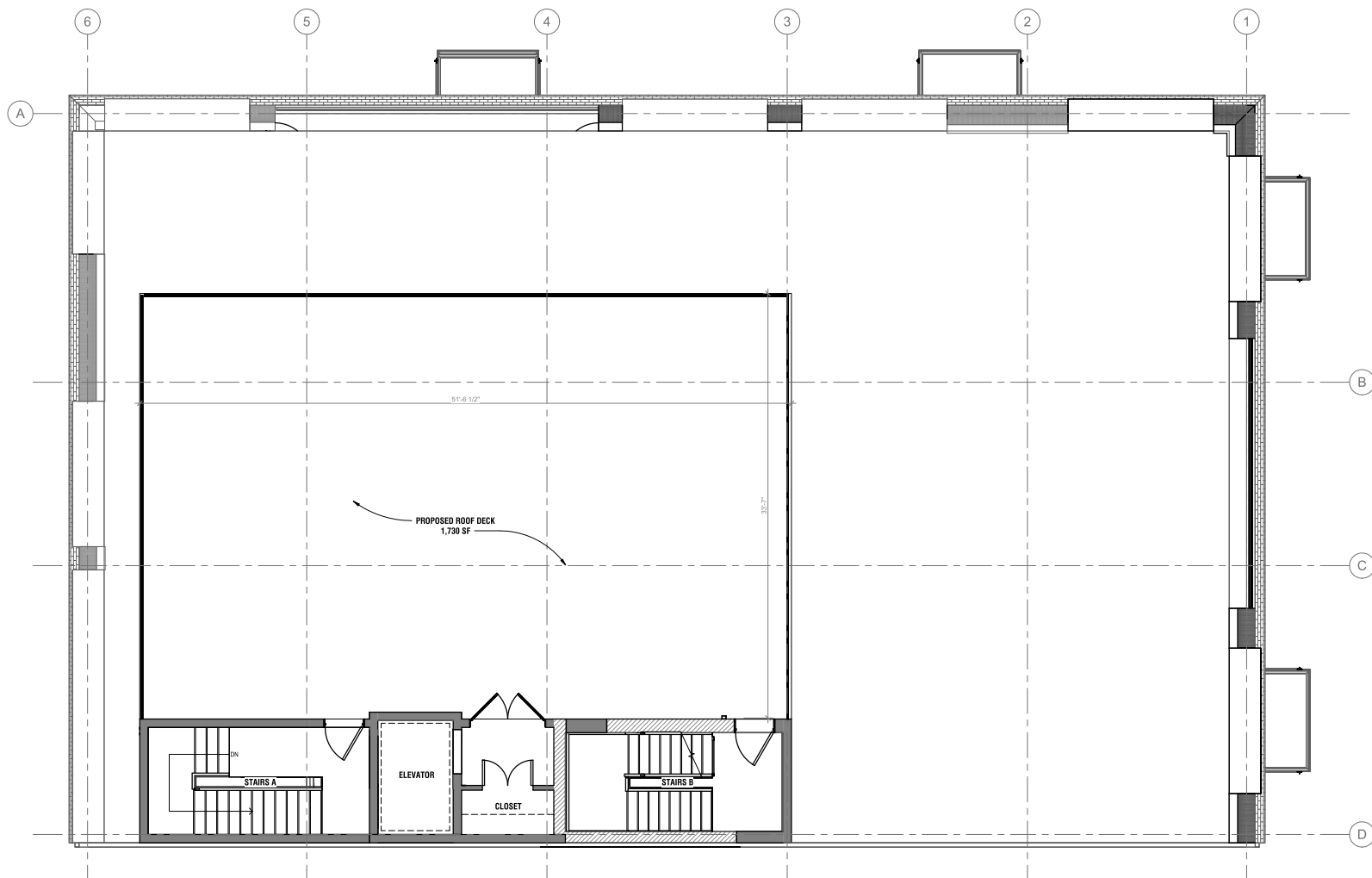
SEAL

SHEET NAME  
PROPOSED FIFTH  
FLOOR

SCALE  
1/4" = 1'-0"

SHEET NO.

**A103**



1 PROPOSED ROOF PLAN  
1/4" = 1'-0"

OFFICIAL STAMPS

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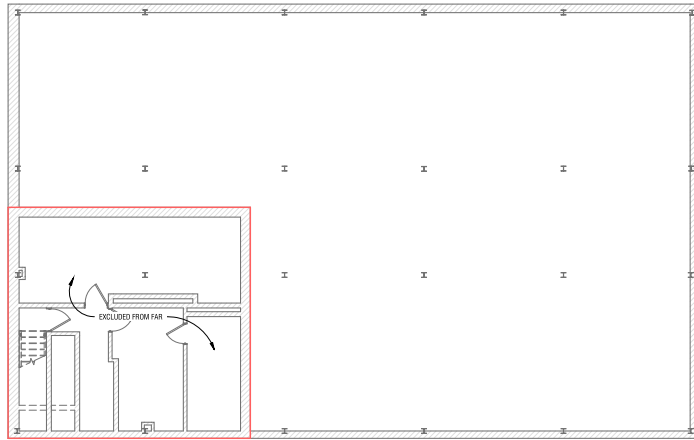
SHEET NAME  
PROPOSED ROOF  
PLAN

SCALE  
1/4" = 1'-0"

SHEET NO.

A104





1 BASEMENT PLAN - FAR  
1/8" = 1'-0"



3 TYPICAL FLOOR PLAN (2-4) FAR  
1/8" = 1'-0"



2 FIRST FLOOR PLAN - FAR  
1/8" = 1'-0"

732 N Washington St - Existing FAR Calculations		FAR - Deductions			Non FAR Total	Net Floor Area
Description	Gross Building Area	Non FAR - Common	Non FAR - Bathrooms	Non FAR - HVAC		
Basement	1,038	-	-	1,038	1,038	-
First Floor	5,525.36	300.00	100.00	62.62	462.62	5,062.74
Typical Floor ( 2nd, 3rd & 4th)	5425.25 (x3)	300.00	100.00	80.02	1,440.06	14,835.75
Fifth Floor	5,425.25	380.02	50.00	567.42	997.44	4,427.81
Existing Roof Stair	257.63	-	-	-	84	-
<b>Total Gross Building Area</b>	<b>28,264.36</b>	<b>Total SF:</b>			<b>4,022.12</b>	<b>24,326.30</b>
Lot Area:	9,432.48					
Existing Net Floor Area	24,326.30					
Existing FAR:	2.58					

732 N Washington St - Proposed FAR Calculations		FAR Deductions			Non FAR Total	Net Floor Area
Description	Gross Building Area	Non FAR - Common	Non FAR - Bathrooms	Non FAR - HVAC		
Basement	1,038	-	-	1,038	1,038	-
First Floor	5,525.36	520.00	300.00	85.61	905.61	4,619.75
Typical Floor ( 2nd, 3rd & 4th)	5425.25 x3	481.00	316.25	58.70	2,567.85	13,707.90
Fifth Floor	5,178.25	481.00	316.25	96.92	894.17	4,284.08
Roof Deck	508.63	500.13	-	-	500.13	8.50
<b>Total Gross Building Area</b>	<b>28,521.99</b>	<b>Total SF:</b>			<b>5,906</b>	<b>22,620.23</b>
Lot Area:	9,432.48					
Proposed Net Floor Area :	23,144.02					
Proposed FAR:	2.4					

OFFICIAL STAMPS

JK<sub>A</sub>

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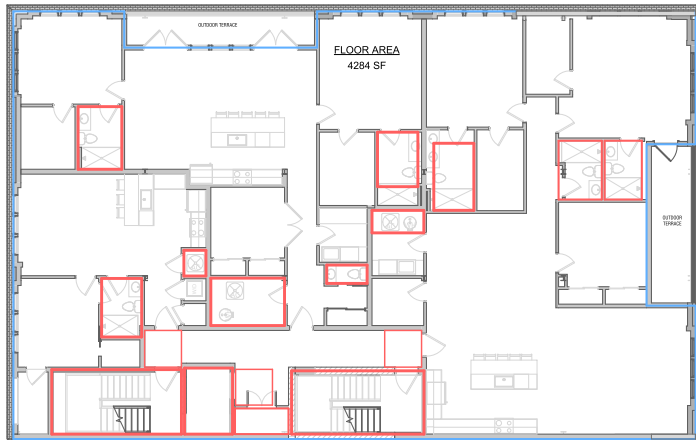
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SHEET NAME  
FAR - CALCULATIONS

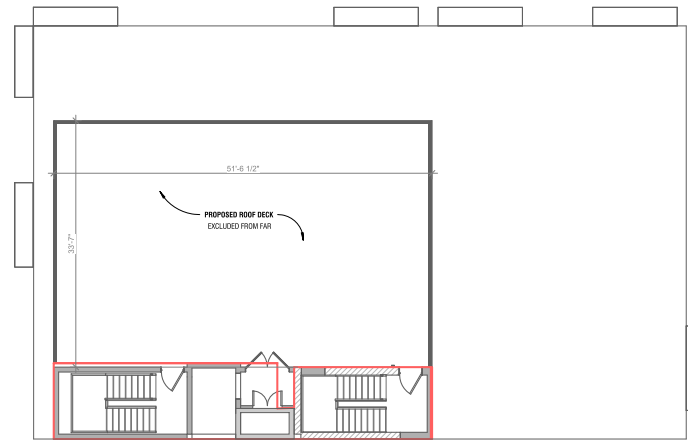
SCALE  
1/8" = 1'-0"

SHEET NO.

A107



1 FIFTH FLOOR PLAN - FAR  
1/8" = 1'-0"



2 ROOF PLAN - FAR  
1/8" = 1'-0"

732 N Washington St - Existing FAR Calculations		FAR - Deductions			Non FAR Total	Net Floor Area
Description	Gross Building Area	Non FAR - Common	Non FAR - Bathrooms	Non FAR - HVAC		
Basement	1,038	-	-	1,038	1,038	-
First Floor	5,525.36	300.00	100.00	62.62	462.62	5,062.74
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Fifth Floor	5,425.25	380.02	50.00	567.42	997.44	4,427.81
Existing Roof Stair	257.63	-	-	-	84	-
<b>Total Gross Building Area</b>	<b>28,264.36</b>	<b>Total SF:</b>			<b>4,022.12</b>	<b>24,326.30</b>
Lot Area:	9,432.48					
Existing Net Floor Area	24,326.30					
Existing FAR:	2.58					

732 N Washington St - Proposed FAR Calculations		FAR Deductions			Non FAR Total	Net Floor Area
Description	Gross Building Area	Non FAR - Common	Non FAR - Bathrooms	Non FAR - HVAC		
Basement	1,038	-	-	1,038	1,038	-
First Floor	5,525.36	520.00	300.00	85.61	905.61	4,619.75
Typical Floor ( 2nd, 3rd & 4th)	5425.25 x3	481.00	316.25	58.70	2,567.85	13,707.90
Fifth Floor	5,178.25	481.00	316.25	96.92	894.17	4,284.08
Roof Deck	508.63	500.13	-	-	500.13	8.50
<b>Total Gross Building Area</b>	<b>28,521.99</b>	<b>Total SF:</b>			<b>5,906</b>	<b>22,620.23</b>
Lot Area:	9,432.48					
Proposed Net Floor Area :	23,144.02					
Proposed FAR:	2.4					

OFFICIAL STAMPS

JK<sub>A</sub>

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SEAL

SHEET NAME

FAR - CALCULATIONS

SCALE  
1/8" = 1'-0"

SHEET NO.

A108



6 January 2026

Members of the Alexandria Planning Commission,

As the Alexandria leadership team for YIMBYs of Northern Virginia, we are excited to see two significant housing developments on today's docket, in addition to three subdivisions and a Zoning Text Amendment that is a step in the right direction towards streamlining the process for office-to-residential conversions, and we ask you to vote yes on all of them. Together, these developments will add 31 desperately needed new homes to Alexandria. Lastly, we'd like to share our perspective on the proposed Green Building Plan.

Docket item 6, 220 and 224 S Peyton Street, will replace a small surface parking lot with eight homes close to the King Street Metro station. The development will also benefit its neighbors with improved sidewalks and new street trees. We would like to note that you are again being asked to approve a parking reduction within a quarter mile of a Metro station in one of our city's most walkable areas. We hope you will legalize parking flexibility to make it easier to build homes like these, in locations where residents have choices in how to get around.

Docket item 7, 732 N Washington and 710 Madison, will convert a 1960s office building in Old Town North to twenty homes. In addition to the fact that a parking reduction is again required for a development within a half mile of a Metro station, we'd like to note that a Special Use Permit is required to build a residential building in the CRMU-X zone. A residential building in a residential zone should not be considered a special use. We hope you will consider removing this requirement, and more broadly, simplifying our zones so that housing is legal everywhere in Alexandria. Lastly, we'd like to comment on the map in the staff report showing that this development will be the sixth height increase to the Old Town North Small Area Plan, which is less than ten years old. We hope this will inform height limits that are appropriate for the housing our city needs in future planning processes, including the Duke Street plan.

Docket item 8, the zoning text amendment, is a step in the right direction towards simplifying the approval process for office-to-residential conversions. We enthusiastically support formalizing staff's interpretation to allow existing noncomplying buildings to convert to residential use. The new SUP process is a step in the right direction towards streamlining the regulatory complexity around office-to-residential conversions. We hope you will approve the proposed amendment, and then make further changes to our zoning code to allow more office-to-residential conversions to be approved by-right rather than requiring City Council approval for an SUP. This could include increasing residential FAR limits to match commercial limits in zones where they are not equal, and exempting office-to-residential conversions from requirements like open space and setbacks that do not apply to the existing office building.

Lastly, we'd like to share our thoughts on Docket Item 9, the proposed Green Building Plan. We unfortunately missed the initial public engagement last year, and we wish we had a clearer understanding of how the cost for building housing compares to the current policy. If the proposed policy will lower the cost of building homes as compared to the current policy in virtually all cases, then



we enthusiastically support it. If there are cases where it would increase the cost of building homes, then we urge caution and consideration of how the cost of important environmental requirements can be offset to ensure projects remain feasible in Alexandria itself. Any policy that aims to make buildings in Alexandria more sustainable must also acknowledge that any multifamily housing building within the City of Alexandria is inherently sustainable compared to the alternative. When a project cannot be built affordably in Alexandria it does not simply stop existing; that demand of housing is instead fulfilled elsewhere. This drives regional housing sprawl, increasing emissions as more people are forced to live in jurisdictions that are both A) more car-reliant and B) do not require projects to be built as sustainably. Reducing "Alexandria's" greenhouse gas emissions by displacing them into Fairfax or Prince William Counties is not a good solution. We are particularly concerned by the projected high costs for multifamily low-rise buildings, which the plan estimates at \$10 per square foot. More care should be given to reducing the costs of or allowing more leeway for this housing type.

In addition to that broader consideration, we suggest two specific changes to the Plan:

- 1) The plan mandates a certain percentage of required parking spaces be compatible for electric vehicles. The plan should instead: A) call for the elimination of parking mandates, since mandating more parking spaces than people actually need is bad for the environment, and B) shift how EV spaces are required by applying the percent to a project's number of *built* parking spaces rather than the *mandated* spaces. This disentangles the Green Building Plan from the current parking mandate policy, and also avoids penalizing developers for seeking a parking reduction.
- 2) The plan exempts buildings "under 25,000 square feet or with 4 or fewer units." By exempting the most inherently costly and least inherently sustainable form of housing (large single-family homes) while layering requirements on the most naturally affordable ones (multifamily, especially low rise), this creates a perverse incentive that will further encourage builders to focus on luxury houses. It is our understanding that all buildings over 3,000 square feet require a DSP, in theory allowing them to be subjected to this plan. Every single family home in Alexandria over 3,000 square feet costs at least \$1 million. It seems fair to apply higher standards to these luxury homes, and balance this by easing restrictions on the most-impacted and more affordable typologies.

We hope Alexandria will continue to welcome new homes of all types, all price points, and in all parts of our city to address our regional housing crisis and make our city better for everyone.

Phoebe Coy, Alex Goyette, Peter Sutherland, Stephanie Elms and Yasir Nagi  
YIMBYs of Northern Virginia Alexandria leads

**From:** [Todd Perry](#)  
**To:** [PlanComm](#)  
**Subject:** [EXTERNAL]Resident support for docket items 6 and 7 in today's meeting  
**Date:** Tuesday, January 6, 2026 7:58:05 AM

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[You don't often get email from toddperry99@gmail.com. Learn why this is important at <https://aka.ms/LearnAboutSenderIdentification> ]

Good morning,

My name is Todd Perry, and I am a renter in Old Town. I am writing to show my support for docket items 6 and 7 in today's meeting. Our rent is way too high and any additional housing will help stabilize rents long term.

Thank you,

Todd Perry  
Sent from my iPhone

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